



LAND DEVELOPMENT CODE (LDC) AMENDMENT

PETITION
PL20190001927

SUMMARY OF AMENDMENT

This LDC amendment (LDCA) serves to implement several policies of the recently adopted Golden Gate Area Master Plan (GGAMP) by creating the Golden Gate Parkway Overlay District (GGPOD). The GGPOD will be comprised of two new subdistricts—the Activity Center subdistrict (GGPOD-AC) and the Downtown subdistrict (GGPOD-DT). The GGPOD will supersede the provisions of the Golden Gate Downtown Center Overlay District and the Golden Gate Parkway Professional Office Commercial Overlay.

ORIGIN
Board of County
Commissioners (Board)

HEARING DATES

Board TBD
CCPC TBD
DSAC TBD
DSAC-LDR 12-17-2019

LDC SECTIONS TO BE AMENDED

- 1.08.01 Abbreviations
- 2.03.07 Overlay Zoning Districts
- 2.05.01 Density Standards and Housing Types
- 4.02.26 Golden Gate Parkway Activity Center Overlay (GGPACO) Building, Development, and Site Design Standards
- 4.02.37 Design Standards for Development in the Golden Gate Downtown Center Commercial Overlay District (GGDCCO)
- 5.05.01 Businesses Serving Alcoholic Beverages
- 5.06.02 Development Standards for Signs within Residential Districts
- 10.03.06 Public Notice and Required Hearings for Land Use Petitions

ADVISORY BOARD RECOMMENDATIONS

DSAC-LDR
TBD

DSAC
TBD

CCPC
TBD

BACKGROUND: The GGAMP was adopted on September 24, 2019, and this LDCA implements the changes to the LDC that are recommended by GGAMP. These four GGAMP policies are itemized as follows:

Policy 2.1.3

Within two years, Collier County shall create development standards to guide the transformation of Golden Gate Parkway and the Activity Center into destinations that are convenient, pleasant and safe for pedestrians and cyclists, and maintain strong connections to transit service.

Policy 2.1.5

Collier County shall discourage new land uses along Golden Gate Parkway and within the Activity Center that impede pedestrian activity and are solely auto-oriented, such as car washes, storage facilities, auto dealerships and drive throughs. Land development regulations will be initiated within two years to ensure compatibility of auto-oriented uses within the pedestrian environment.

Policy 2.2.2

Within two years of adoption, Collier County shall initiate a community renewal plan to include economic development strategies, urban design schemes, and infrastructure improvements. Renewal plans shall be in concert with the vision of Golden Gate City and promote vibrant urbanism, improve aesthetics and support walkability.

Policy 2.2.3

Within two years of adoption, Collier County shall initiate the involvement of Golden Gate City residents and business owners to consider amendments to the Land Development Code to support and implement residential and commercial redevelopment and renewal initiatives. Amendments shall include incentives for remodeling and renovation by creating criteria and standards for variances and/or deviations.

This LDCA establishes the Golden Gate Parkway Overlay District (GGPOD) and its two subdistricts—the Activity Center subdistrict (GGPOD-AC) and the Downtown subdistrict (GGPOD-DT). The GGPOD is intended to work in tandem with the County’s Golden Gate City Economic Development Zone (see Exhibit B). The creation of the GGPOD eliminates all provisions of the Golden Gate Parkway Professional Office Commercial Overlay in LDC sections 2.03.07 F. and 4.02.26. The GGPOD also eliminates all provisions of the Golden Gate Downtown Center Commercial Overlay District in LDC sections 2.03.7 O. and 4.02.37. Once this LDCA is adopted, the Official Zoning Atlas maps will be updated to include the “GGPOD” label for each affected parcel within the GGPOD.

The main components of the GGPOD are generally summarized as follows: purpose and intent, applicability, and conflict sections; new definitions; the boundary of the GGPOD and the creation of two subdistricts; a table containing new principal uses that are allowed in the GGPOD; new development standards, including off-street parking, that are specific to the GGPOD; new performance standards for Employment uses; and a specific relief process to allow deviations from the GGPOD or various standards of the LDC. One main facet of the GGPOD is to incentivize redevelopment within the Golden Gate community by proposing a host of new advanced manufacturing uses. This was done as a collaborative effort with staff from the County’s Office of Business and Economic Development. These advanced manufacturing uses are not allowed under the current zoning or existing overlays. Operators of these targeted industries will need to demonstrate compliance with the new performance standards that have been specifically drafted for them. Lastly, the GGPOD introduces new development standards that are aimed at promoting vibrant urbanism, improving aesthetics, and supporting walkability.

FISCAL & OPERATIONAL IMPACTS

There are no anticipated fiscal or operational impacts to the County other than the typical advertising expenses associated with public notice of the proposed LDCA. Property owners requesting deviations will have to pay for the application fee.

GMP CONSISTENCY

The proposed changes to the LDC are consistent with the updates in the recently adopted GMP amendment.

EXHIBITS: A) Proposed Text to AC; B) Ordinance 2018-56; C) Golden Gate Community Roadways Beautification Master Plan; and D) ISO/ASTM 52900 (protected)

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Amend the LDC as follows:

1.08.01 – Abbreviations

* * * * *

GGAMP	Golden Gate Area Master Plan
<u>GGPOD</u>	<u>Golden Gate Parkway Overlay District</u>
<u>GGPOD-AC</u>	<u>Golden Gate Parkway Overlay District-Activity Center subdistrict</u>
<u>GGPOD-DT</u>	<u>Golden Gate Parkway Overlay District-Downtown subdistrict</u>
GGPPOCO	Golden Gate Parkway Professional Office Commercial Overlay District
GMP	Collier County Growth Management Plan

#

2.03.07 - Overlay Zoning Districts

* * * * *

F. Golden Gate Parkway Overlay District (GGPOD)

1. Purpose and intent. The purpose and intent of these provisions is that they shall be utilized to implement the policy direction and community vision of the GGAMP in concert with the County's Golden Gate City Economic Development Zone, Ordinance 2018-56, to encourage the following:

- a. The concentration of commercial, residential, and institutional uses to serve as a focal point of the community;
- b. A mixed use development pattern;
- c. Economic development through an expanded list of permitted and conditional uses;
- d. An improved public pedestrian network and experience through the mixing of land uses, new design standards, and limiting the proliferation of auto-oriented uses;
- e. Flexibility when remodeling and renovating buildings and structures through a deviation process;
- f. Quality and consistency in design while maintaining flexibility for market responsiveness; and
- g. Compatibility of projects within the GGPOD and in relation to residentially-zoned properties located outside the GGPOD.

- 1
- 2 2. Applicability.
- 3
- 4 a. The regulations of this LDC section shall apply to all properties identified
- 5 as shown in Illustration 1 of LDC section 2.03.07 F.5.b.
- 6
- 7 b. Property owners within the GGPOD, including PUDs, may establish uses,
- 8 densities, and intensities in accordance with the LDC regulations of the
- 9 underlying zoning classification or may elect to develop/redevelop under
- 10 the provisions of the applicable GGPOD Subdistrict. In either instance, the
- 11 GGPOD site development standards as provided for in LDC section
- 12 4.02.26 shall apply. However, the density of each lot shall not exceed the
- 13 maximum allowed pursuant to the GGAMP and/or the underlying zoning
- 14 district.
- 15
- 16 c. The provisions of the GGPOD shall be available to all Planned Unit
- 17 Developments (PUDs) that existed prior to the effective date of this
- 18 ordinance, and properties with Conditional Uses (CU) approved prior to the
- 19 effective date of this ordinance (placeholder for date), including
- 20 amendments or boundary changes to these PUDs and Conditional Use
- 21 properties. The provisions of the GGPOD shall also be available to any
- 22 property currently located within the GGPOD that is rezoned to PUD.
- 23
- 24 3. Conflict. In the event of conflict between the uses allowed in the GGPOD
- 25 compared with the underlying zoning district or the specific development or design
- 26 standards contained herein compared with the development or design standards
- 27 of the LDC, including definitions, the standards of the GGPOD shall prevail.
- 28
- 29 4. Definitions. The following definitions shall be used exclusively to the GGPOD:
- 30
- 31 *Additive manufacturing:* The process of joining materials to make parts from 3D
- 32 model data, usually layer upon layer, as opposed to subtractive manufacturing and
- 33 formative manufacturing methodologies. Also see International Organization for
- 34 Standardization/American Society for Testing and Materials (ISO/ASTM)
- 35 52900:2015.
- 36
- 37 *Advanced manufacturing:* The use of innovative technology to improve products
- 38 or processes. The advanced manufacturing entity makes extensive use of
- 39 computer, high precision, and information technologies integrated with a high-
- 40 performance workforce in a production system capable of furnishing a
- 41 heterogenous mix of products in small or large volumes with both the efficiency of
- 42 mass-production and the flexibility of custom manufacturing in order to respond
- 43 quickly to consumer demands.
- 44
- 45 *Food truck:* Known as a mobile food dispensing vehicle by the FAC, a food truck
- 46 is a type of vehicle-mounted public food service establishment that is either self-
- 47 propelled or otherwise movable from place to place and includes self-contained

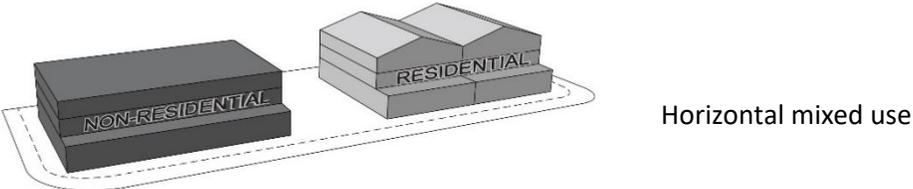
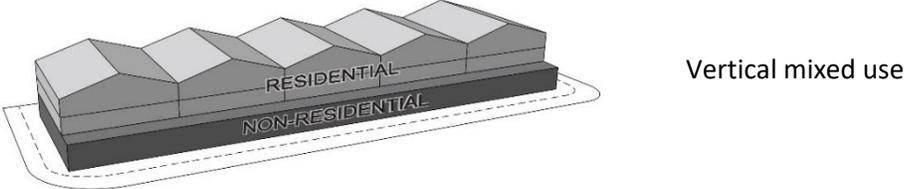
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1 utilities, such as gas, water, electricity, and liquid waste disposal. Food trucks shall
2 not include self-propelled watercraft, vessels, rafts, or barges. Food trucks are
3 classified as SIC Code 5963 Direct Selling Establishments. Also see FAC 61C-
4 1.002(5)(a)2.

5
6 Food truck park: A site that is designed to provide electrical, water, sewer
7 connections for food trucks and associated facilities such as, but not limited to,
8 seating, bathrooms, and drinking or bar area that is accessory to the principal use,
9 which is food sales. A food truck park shall also include off-street parking for
10 visiting patrons.

11
12 Layer: In additive manufacturing, “layer” means material laid or spread out to
13 create a surface.

14
15 Mixed use: A single development project with a residential component and a
16 nonresidential component. The mix of uses may be arranged horizontally
17 (separate buildings with a common development plan) or vertically (in the same
18 building), or some combination of both. Examples include but are not limited to
19 the following:



34 (For illustrative purposes only)

35
36 (Images to be added)

37
38 Park: A use of land that is open to the general public and designed principally for
39 recreational purposes and often includes but is not limited to recreational
40 equipment or amenities, existing or planted vegetation, fountains, seating areas,
41 or other open space features.

42
43 Parking lot: A ground-level area utilized for parking spaces accessible from a right-
44 of-way or other means of legal access and usually adjacent to the use it serves.
45

1 Parking structure: A multi-level parking facility utilized for parking spaces that serve
2 establishments within walking distance of the structure. The structure may or may
3 not be adjacent to the establishments it serves.

4
5 Part: In additive manufacturing, “part” means joined material forming a functional
6 element that could constitute all or a section of an intended product.

7
8 Streetwall: An opaque freestanding wall aligned with the facade of an adjacent
9 building with the purpose of masking off-street parking from the street.

10
11 Targeted industry: A qualified targeted industry business as defined by Section
12 288.106, Florida Statutes, and such businesses or industries identified by the
13 Board of County Commissioners.

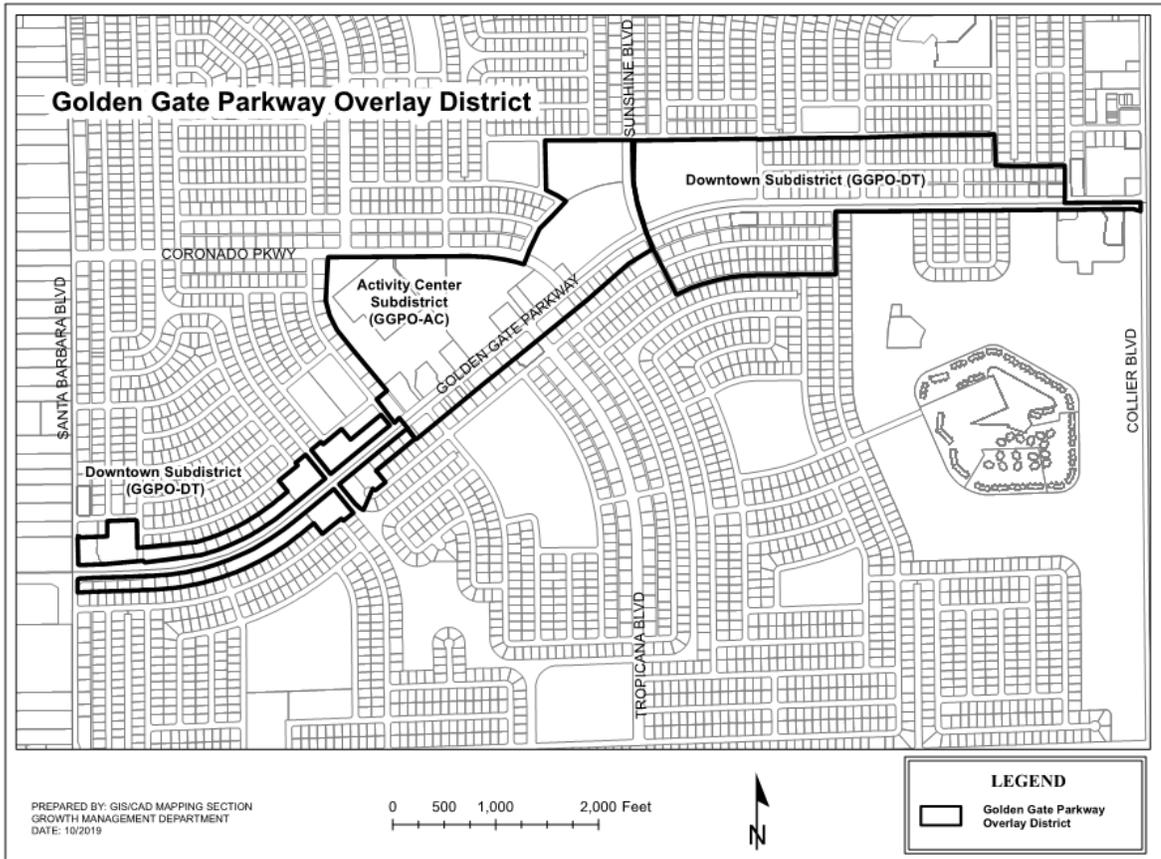
14
15 Tasting room: An indoor space within a beverage manufacturing establishment
16 that is dedicated to the retail sale and consumption of food and beverages,
17 including alcoholic beverages, produced on-site by a beverage manufacturing
18 establishment. In addition to food and beverages manufactured on-site, a tasting
19 room may serve food and beverages, including alcoholic beverages, that are
20 manufactured off-site and/or by a different establishment.

21
22 5. Establishment of subdistricts.

23
24 a. Purpose and Intent. The Golden Gate Parkway Overlay District-Activity
25 Center subdistrict (GGPOD-AC) and the Golden Gate Parkway Overlay
26 District-Downtown subdistrict (GGPOD-DT) are established to define
27 standards that address differences in desired residential densities, non-
28 residential intensities, and allowable uses along Golden Gate Parkway
29 corridor to the west and east of the Activity Center. Both are created to
30 promote a pedestrian-friendly environment in close proximity to
31 opportunities for shopping, dining, employment, and entertainment with a
32 mix of residential and commercial uses.

33
34 b. Boundaries of GGPOD and subdistricts. The boundaries of the GGPOD
35 and subdistricts are identified in Illustration 1 below:
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1



(map to be added)

Illustration 1 - GGPOD with subdistricts

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6. Table 1. Table of Uses. The Table of Uses identifies uses as permitted uses (P) or conditional uses (C). Blank cells indicate that a use is not allowed in the corresponding subdistrict.

<u>Use Category</u>	<u>Activity Center Subdistrict (GGPOD-AC)</u>	<u>Downtown Subdistrict (GGPOD-DT)¹</u>
<u>Residential (R)</u>		
<u>Any principal or accessory use listed as permitted in the underlying zoning district.</u>	<u>P</u>	<u>P</u>
<u>Any principal or accessory use listed as conditional in the underlying zoning district.</u>	<u>C</u>	<u>C</u>
<u>Artist village.</u>	<u>P</u>	<u>P</u>
<u>Multifamily dwellings, including townhouses.</u>	<u>P</u>	<u>P</u>

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<u>Any other principal residential use which is comparable in nature with the foregoing uses, consistent with the list of permitted uses and the purpose and intent of the GGPOD, as determined by the County Manager or designee.</u>	<u>P or C</u>	<u>P or C</u>
<u>Mixed Use (M)²</u>		
<u>A single development project comprised of an allowable residential use from this table in combination with an allowable non-residential use from this table.</u>	<u>P</u>	<u>P</u>
<u>Commercial (C)²</u>		
<u>Accounting services (8721).</u>	<u>P</u>	<u>P</u>
<u>Adjustment and collection services (7322).</u>	<u>P</u>	<u>P</u>
<u>Advertising agencies (7311).</u>	<u>P</u>	<u>P</u>
<u>Amusement and recreational services, indoor (7999 - martial arts, yoga and gymnastics instruction, and recreation involvement physical fitness exercise only).</u>	<u>P</u>	<u>P</u>
<u>Animal specialty services, limited to pet grooming with no outside kenneling (0752).</u>	<u>P</u>	<u>P</u>
<u>Apparel and accessory stores (5611-5699).</u>	<u>P</u>	<u>P</u>
<u>Architectural services (8712).</u>	<u>P</u>	<u>P</u>
<u>Auditing (8721).</u>	<u>P</u>	<u>P</u>
<u>Auto and home supply stores (5531).</u>	<u>P</u>	<u>P</u>
<u>Automobile parking, automobile parking garages and parking structures (7521 – shall not be construed to permit the activity of “tow-in parking lots”).</u>	<u>P³</u>	<u>P³</u>
<u>Banks and credit unions (6021-6062).</u>	<u>P</u>	<u>P</u>
<u>Barber shops (7241).</u>	<u>P</u>	<u>P</u>
<u>Beauty shops (7231).</u>	<u>P</u>	<u>P</u>
<u>Bookkeeping services (8721).</u>	<u>P</u>	<u>P</u>
<u>Building cleaning and maintenance services (7349).</u>	<u>P</u>	<u>P</u>
<u>Business associations (8611).</u>	<u>P</u>	<u>P</u>
<u>Business consulting services (8748).</u>	<u>P</u>	<u>P</u>
<u>Business credit institutions (6153-6159).</u>	<u>P</u>	<u>P</u>
<u>Business repair service.</u>	<u>P</u>	<u>P</u>
<u>Carpet and upholstery cleaning (7217).</u>	<u>P</u>	<u>P</u>
<u>Child day care services (8351).</u>	<u>P</u>	<u>P</u>
<u>Churches.</u>	<u>P</u>	<u>P</u>
<u>Civic, social, and fraternal associations (8641).</u>	<u>P</u>	<u>P</u>
<u>Commercial art and graphic design (7336).</u>	<u>P</u>	<u>P</u>
<u>Commercial photography (7335).</u>	<u>P</u>	<u>P</u>
<u>Computer programming, data processing, rental, leasing, repair and other services (7371-7373, 7375-7379).</u>	<u>P</u>	<u>P</u>
<u>Computer and computer software stores (5734).</u>	<u>P</u>	<u>P</u>

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<u>Credit reporting services (7323).</u>	<u>P</u>	<u>P</u>
<u>Department stores (5311).</u>	<u>P</u>	<u>P</u>
<u>Direct mail advertising services (7331).</u>	<u>P</u>	<u>P</u>
<u>Disinfecting and pest control services (7342).</u>	<u>P</u>	<u>P</u>
<u>Drinking places (5813).</u>	<u>P</u>	<u>C</u>
<u>Drug stores (5912).</u>	<u>P</u>	<u>P</u>
<u>Eating establishments and places (5812 except commercial use employing drive-up, drive-in, or drive-through delivery of goods and/or services).</u>	<u>P</u>	<u>P</u>
<u>Electrical and electronic repair shop (7629).</u>	<u>P</u>	<u>P</u>
<u>Employment agencies (7361).</u>	<u>P</u>	<u>P</u>
<u>Engineering services (8711).</u>	<u>P</u>	<u>P</u>
<u>Equipment rental and leasing (7359), not including heavy construction equipment.</u>	<u>P</u>	<u>P</u>
<u>Essential services, see LDC section 2.01.03.</u>	<u>P</u>	<u>P</u>
<u>Food stores (groups 5411-5499).</u>	<u>P</u>	<u>P</u>
<u>Food truck park or lunch wagons, mobile-retail (5963).</u>	<u>P</u>	<u>P</u>
<u>Funeral service and crematories (7261).</u>	<u>P</u>	<u>P</u>
<u>Garment pressing, and agents for laundries and drycleaners (7212).</u>	<u>P</u>	<u>P</u>
<u>General merchandise stores (5331-5399).</u>	<u>P</u>	<u>P</u>
<u>Glass stores (5231).</u>	<u>P</u>	<u>P</u>
<u>Hardware stores (5251).</u>	<u>P</u>	<u>P</u>
<u>Health services, offices, and clinics (8011-8049).</u>	<u>P</u>	<u>P</u>
<u>Health services (8071, 8092, and 8099).</u>	<u>P</u>	<u>C</u>
<u>Holding and other investment offices (6712-6799).</u>	<u>P</u>	<u>P</u>
<u>Home furniture and furnishing stores (5712-5719).</u>	<u>P</u>	<u>P</u>
<u>Home health care services (8082).</u>	<u>P</u>	<u>P</u>
<u>Hotel, excluding single room occupancy hotels.</u>	<u>P</u>	<u>C</u>
<u>Household appliance stores (5722).</u>	<u>P</u>	<u>P</u>
<u>Insurance carriers, agents and brokers (6311-6399, 6411).</u>	<u>P</u>	<u>P</u>
<u>Job training and vocational rehabilitation services (8331).</u>	<u>P</u>	<u>P</u>
<u>Labor unions (8631).</u>	<u>P</u>	<u>P</u>
<u>Landscape architects, consulting and planning (0781).</u>	<u>P</u>	<u>P</u>
<u>Laundry, cleaning, and garment services, except no coin-operated laundries or drycleaners (7219)</u>	<u>P</u>	<u>P</u>
<u>Legal services (8111).</u>	<u>P</u>	<u>P</u>
<u>Libraries (8231).</u>	<u>P</u>	<u>P</u>
<u>Linen supply (7213)</u>	<u>P</u>	<u>P</u>
<u>Loan brokers (6163).</u>	<u>P</u>	<u>P</u>
<u>Management services (8741, 8742).</u>	<u>P</u>	<u>P</u>
<u>Medical equipment rental and leasing (7352).</u>	<u>P</u>	<u>P</u>

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<u>Membership organizations - miscellaneous (8699).</u>	<u>P</u>	<u>P</u>
<u>Mortgage bankers and loan correspondents (6162).</u>	<u>P</u>	<u>P</u>
<u>Museums and art galleries (8412).</u>	<u>P</u>	<u>P</u>
<u>Musical instrument stores (5736).</u>	<u>P</u>	<u>P</u>
<u>Paint stores (5231).</u>	<u>P</u>	<u>P</u>
<u>Park.</u>	<u>P</u>	<u>P</u>
<u>Personal credit institutions (6141).</u>	<u>P</u>	<u>P</u>
<u>Personal services - miscellaneous (7299).</u>	<u>P</u>	<u>P⁵</u>
<u>Personnel supply services (7363, except labor pools).</u>	<u>P</u>	<u>P</u>
<u>Photocopying and duplicating services (7334).</u>	<u>P</u>	<u>P</u>
<u>Photofinishing laboratories (7384).</u>	<u>P</u>	<u>P</u>
<u>Photographic studios, portrait (7221).</u>	<u>P</u>	<u>P</u>
<u>Physical fitness facilities (7991).</u>	<u>P</u>	<u>P</u>
<u>Political organizations (8651).</u>	<u>P</u>	<u>P</u>
<u>Professional membership organizations (8621).</u>	<u>P</u>	<u>P</u>
<u>Public relations services (8743).</u>	<u>P</u>	<u>P</u>
<u>Radio, television, and consumer electronics stores (5731).</u>	<u>P</u>	<u>P</u>
<u>Radio, television, and publishers advertising representatives (7313).</u>	<u>P</u>	<u>P</u>
<u>Record and prerecorded tape stores (5735).</u>	<u>P</u>	<u>P</u>
<u>Real estate (6512-6514, 6519, 6531-6553, 6541).</u>	<u>P</u>	<u>P</u>
<u>Retail, miscellaneous (5921-5963 and 5992-5999, excluding retail firearm and ammunition sales).</u>	<u>P</u>	<u>P</u>
<u>Retail nurseries, lawn and garden supply stores (5261).</u>	<u>P</u>	<u>P</u>
<u>Schools, vocational (8243-8299).</u>	<u>P</u>	<u>P</u>
<u>Secretarial and court reporting services (7338).</u>	<u>P</u>	<u>P</u>
<u>Security and commodity brokers, dealers, exchanges, and services (6211-6289).</u>	<u>P</u>	<u>P</u>
<u>Shoe repair shops or shoeshine parlors (7251).</u>	<u>P</u>	<u>P</u>
<u>Surveying services (8713).</u>	<u>P</u>	<u>P</u>
<u>Tax return preparation services (7291).</u>	<u>P</u>	<u>P</u>
<u>Travel agencies (4724, no other transportation services)</u>	<u>P</u>	<u>P</u>
<u>United States Postal Service (4311, except major distribution center)</u>	<u>P</u>	<u>P</u>
<u>Veterinary services (0742, excluding outside kenneling)</u>	<u>P</u>	<u>P</u>
<u>Wallpaper stores (5231)</u>	<u>P</u>	<u>P</u>
<u>Watch, clock and jewelry repair (7631)</u>	<u>P</u>	<u>P</u>
<u>Employment (E)^{2,4}</u>		
<u>Aircraft and parts (3721-3728).</u>	<u>P</u>	

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<u>Apparel and other finished products (2311-2399), limited to advanced manufacturing as defined in LDC section 1.08.02.</u>	<u>P</u>	
<u>Business services (7374)</u>	<u>P</u>	<u>C</u>
<u>Communications and electronic equipment (3661-3668, 3670-3678, 3680-3699).</u>	<u>P</u>	<u>C</u>
<u>Drugs and medicine (2833-2836)</u>	<u>P</u>	
<u>Electronic and other electrical equipment manufacturing (3629, 3669-3699, 3679)</u>	<u>P</u>	
<u>Fabricated metal products (3411-3499), limited to advanced manufacturing as defined in LDC section 1.08.02.</u>	<u>P</u>	
<u>Food and kindred products (2021-2099), limited to advanced manufacturing as defined in LDC section 1.08.02.</u>	<u>P⁶</u>	<u>C⁶</u>
<u>Furniture and fixtures (2511-2599), limited to advanced manufacturing as defined in LDC section 1.08.02.</u>	<u>P</u>	
<u>Industrial and commercial machinery and computer equipment (3571-3579)</u>	<u>P</u>	
<u>Instrument manufacturing, limited to medical equipment (3812, 3822-3829, 3841-3845, 3851).</u>	<u>P</u>	
<u>Measuring, analyzing, and controlling instruments; photographic, medical, and optical goods; watches and clocks (3812, 3822-3829, 3841-3851).</u>	<u>P</u>	
<u>Miscellaneous manufacturing (3911-3992, 3994-3999), limited to advanced manufacturing as defined in LDC section 1.08.02.</u>	<u>P</u>	
<u>Movie production, limited to motion picture and video production (7812-7819).</u>	<u>P</u>	
<u>Perfumes, cosmetics, and other toilet preparations (2844).</u>	<u>P</u>	
<u>Textile mill products (2211-2299), limited to advanced manufacturing as defined in LDC section 1.08.02.</u>	<u>P</u>	
<u>Transportation equipment (3751).</u>	<u>P</u>	
<u>Qualified targeted industry business as defined by Florida Statute 288.106.</u>	<u>P</u>	
<u>Nonresidential Uses²</u>		
<u>Any principal or accessory use listed as permitted in the underlying zoning district.</u>	<u>P</u>	<u>P</u>
<u>Any principal or accessory use listed as conditional in the underlying zoning district.</u>	<u>C</u>	<u>C</u>
<u>Any other non-residential use which is comparable in nature with the foregoing uses, consistent with the list of permitted uses and the purpose and intent of the</u>	<u>P</u>	<u>P</u>

<u>GGPOD, as determined by the County Manager or designee.</u>		
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Notes:

¹ No commercial use within the GGPOD-DT shall utilize drive-up, drive-in, or drive-through delivery of goods or services. In addition, there shall be no automatic food and drink vending machines located exterior to a building.

² Pollution control. Any discharge from industrial, commercial, or manufacturing processes to a stormwater or surface water management system is prohibited. Waste water from any industrial, commercial, or manufacturing process must be contained within the facility or disposed of through the Collier County Water-Sewer District's wastewater collection system pursuant to the Collier County Industrial Pretreatment Ordinance, (Ord. No. 2003-18, as amended).

³ Excludes surface parking lots, except if approved as a phased project approved with a parking structure.

⁴ See LDC section 4.02.26 D. for performance standards specific to Employment uses.

⁵ Personal services in the GGPOD-DT Subdistrict excludes Escort service, Steam baths, and Turkish baths.

⁶ The size and use of a tasting room shall be subordinate and accessory to the principal use, which is a beverage manufacturing establishment (2082-2087).

~~F. Golden Gate Parkway Professional Office Commercial Overlay (GGPPOCO).~~

~~1. The provisions of the "GGPPOCO" district are intended to provide Golden Gate City with a viable professional office commercial district. The professional office commercial district has two (2) purposes. (1), to serve as a bonafide entry way into Golden Gate City. (2), to provide a community focal point and sense of place. The uses permitted within this district are generally low intensity, office development which minimize vehicular traffic, provide suitable landscaping, control ingress and egress, and ensure compatibility with abutting residential districts.~~

~~2. These regulations apply to properties north and south of Golden Gate Parkway, starting at Santa Barbara Boulevard and extending eastward to 52nd Terrace S.W. in Golden Gate City as measured perpendicularly from the abutting right-of-way for a distance of approximately 3,600 feet more or less and consisting of approximately 20.84 acres. These properties are identified on Map two (2) of the Golden Gate Area Master Plan. Except as provided in this regulation, all other use,~~

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~~dimensional, and development requirements shall be as required in the underlying zoning categories.~~

~~a. Permitted Uses.~~

- ~~1. Accounting (8721).~~
- ~~2. Adjustment and collection services (7322).~~
- ~~3. Advertising agencies (7311).~~
- ~~4. Architectural services (8712).~~
- ~~5. Auditing (8721).~~
- ~~6. Banks and credit Unions (6021-6062).~~
- ~~7. Bookkeeping services (8721).~~
- ~~8. Business associations (8611).~~
- ~~9. Business consulting services (8748).~~
- ~~10. Business credit institutions (6153—6159).~~
- ~~11. Commercial art and graphic design (7336).~~
- ~~12. Commercial photography (7335).~~
- ~~13. Computer programming services (7371).~~
- ~~14. Computer programming, processing, data preparation, information retrieval, facilities management, and miscellaneous services (7371, 7374—7376, 7379).~~
- ~~15. Credit reporting services (7323).~~
- ~~16. Direct mail advertising services (7331).~~
- ~~17. Eating places (5812 except carry-out restaurants; contract feeding; dinner theaters; drive-in and drive-through restaurants; fast food restaurants, carry-out; restaurants, fast-food; submarine sandwich shops).~~
- ~~18. Employment Agencies (7361).~~
- ~~19. Engineering services (8711).~~
- ~~20. Health services, offices and clinics (8011—8049).~~
- ~~21. Holding and other investment offices (6712—6799).~~
- ~~22. Insurance carrier, agents and brokers (6311—6399, 6411).~~
- ~~23. Legal services (8111).~~
- ~~24. Loan brokers (6163).~~
- ~~25. Management services (8741—8742).~~
- ~~26. Mortgage bankers and loan correspondents (6162).~~
- ~~27. Museums and art galleries (8412).~~
- ~~28. Personal credit institutions (6141).~~
- ~~29. Photographic studios, portrait (7221).~~
- ~~30. Professional Membership organizations (8621).~~
- ~~31. Public administration (9111—9199, 9229, 9311, 9411—9451, 9511—9532, 9611—9661).~~
- ~~32. Public relations services (8743).~~
- ~~33. Radio, television and publishers advertising representatives (7313).~~
- ~~34. Real estate (6512—6514, 6519, 6531—6553).~~
- ~~35. Research, development, and testing services (8732).~~
- ~~36. Secretarial and court reporting services (7338).~~

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- ~~37. Security and commodity brokers, dealers, exchanges, and services (6211—6289).~~
- ~~38. Surveying services (8713).~~
- ~~39. Tax return preparation services (7291).~~
- ~~40. Travel agencies (4724).~~
- ~~41. Veterinary services (0742, excluding outside kenneling).~~
- ~~42. Any other commercial use or professional service which is comparable in nature with the foregoing uses.~~

~~b. Accessory Uses.~~

- ~~1. Uses and structures that are accessory and incidental to the permitted uses.~~

- ~~3. For signage to be located along the Golden Gate Parkway, see sections 5.06.00 of the Collier County Sign Code and the Golden Gate Master Plan.~~

#

O. Reserved. ~~Golden Gate Downtown Center Commercial Overlay District (GGDCCO). Special conditions for properties in the vicinity of Golden Gate Parkway in Golden Gate City, as identified on the Golden Gate Downtown Center Commercial Subdistrict Map of the Golden Gate Area Master Plan and as contained herein.~~

- ~~1. *Applicability.* These regulations apply to properties in Golden Gate City lying north of Golden Gate Parkway, generally bounded by 23rd Avenue SW and 23rd Place SW to the north, 45th Street SW to the west, and 41st Street SW and Collier Boulevard to the east. South of Golden Gate Parkway, these regulations apply to properties bounded by 25th Avenue SW to the south, 47th Street SW to the west, and 44th Street SW to the east. These properties are more precisely identified on the "Golden Gate Downtown Center Commercial Subdistrict" map of the Golden Gate Area Master Plan and as depicted on the applicable official zoning atlas maps. Except as provided in this section and section 4.02.26, all other use, dimensional and development requirements shall be as required or allowed in the underlying zoning districts.~~

- ~~2. *Purpose and Intent.* The purpose and intent of this overlay district (see Downtown Center Commercial Subdistrict Map) is to encourage redevelopment herein in order to improve the physical appearance of the area and create a viable downtown district for the residents of Golden Gate City and Golden Gate Estates. Emphasis shall be placed on the creation of pedestrian-oriented areas, such as outdoor dining areas and pocket parks, that do not impede the flow of traffic along Golden Gate Parkway. Also, emphasis shall be placed on the construction of mixed-use buildings. Residential dwelling units constructed in this overlay district are intended to promote resident-business ownership. The provisions of this overlay district are intended to ensure harmonious development of commercial~~

1 and mixed-use buildings at a pedestrian scale that is compatible with residential
2 development within and outside of the overlay district.
3

4 ~~3. *Aggregation of Properties.* This overlay district encourages the aggregation of~~
5 ~~properties in order to promote flexibility in site design. The types of uses permitted~~
6 ~~within this overlay district are low intensity retail, office, personal services,~~
7 ~~institutional, and residential. Non-residential development is intended to serve the~~
8 ~~needs of residents within the overlay district, surrounding neighborhoods, and~~
9 ~~passersby.~~

10
11 ~~4. *Permitted uses:*~~

12
13 ~~a. Residential uses: As permitted by right in the existing residential zoning~~
14 ~~districts, except as otherwise prohibited by this overlay, when:~~

15
16 ~~1. In an existing owner occupied structure.~~

17
18 ~~2. In an existing non-owner occupied structure, until such time as~~
19 ~~cessation is required by subsection 4.02.37 A.1~~

20
21 ~~b. Residential within a mixed use building~~

22
23 ~~c. Commercial uses:~~

24
25 ~~1. Accounting services (8721).~~

26 ~~2. Adjustment and collection services (7322).~~

27 ~~3. Advertising Agencies (7311).~~

28 ~~4. Apparel & accessory stores (5611-5699).~~

29 ~~5. Architectural services (8712), limited to 5,000 square feet per floor.~~

30 ~~6. Auto and home supply stores (5531).~~

31 ~~7. Barber shops (7241).~~

32 ~~8. Beauty shops (7231).~~

33 ~~9. Building cleaning and maintenance services (7349).~~

34 ~~10. Business associations (8611).~~

35 ~~11. Business consulting services (8748).~~

36 ~~12. Business services – miscellaneous (7397).~~

37 ~~13. Business repair service.~~

38 ~~14. Carpet and upholstery cleaning (7217).~~

39 ~~15. Commercial art and graphic design (7336).~~

40 ~~16. Commercial photography (7335).~~

41 ~~17. Computer programming, data processing, rental, leasing, repair~~
42 ~~and other services (7371-7379).~~

43 ~~18. Computer and computer software stores (5734).~~

44 ~~19. Credit reporting services (7323).~~

45 ~~20. Department stores (5311).~~

46 ~~21. Direct mail advertising services (7331).~~

47 ~~22. Disinfecting and pest control services (7342).~~

- 1 ~~23. Drug stores (5912), limited to 5,000 square feet per floor.~~
- 2 ~~24. Eating establishments and places (5812 except commercial use~~
- 3 ~~employing drive-up, drive-in, or drive-through delivery of goods~~
- 4 ~~and/or services).~~
- 5 ~~25. Electrical and electronic repair shop (7629).~~
- 6 ~~26. Employment agencies (7361).~~
- 7 ~~27. Engineering services (8711), limited to 5,000 square feet per floor.~~
- 8 ~~28. Equipment rental and leasing (7359), not including heavy~~
- 9 ~~construction equipment.~~
- 10 ~~29. Essential services, see sec. 2.01.03; except that law enforcement,~~
- 11 ~~fire, and emergency medical services uses are limited to~~
- 12 ~~administrative offices only.~~
- 13 ~~30. Food stores (groups 5411-5499).~~
- 14 ~~31. Funeral service and crematories (7261).~~
- 15 ~~32. General merchandise stores (5331-5399).~~
- 16 ~~33. Glass stores (5231).~~
- 17 ~~34. Hardware stores (5251).~~
- 18 ~~35. Health services, offices and clinics (8011-8049).~~
- 19 ~~36. Home furniture and furnishing stores (5712-5719).~~
- 20 ~~37. Home health care services (8082).~~
- 21 ~~38. Household appliance stores (5722).~~
- 22 ~~39. Insurance carriers, agents and brokers (6311-6399, 6411).~~
- 23 ~~40. Labor unions (8631).~~
- 24 ~~41. Landscape architects, consulting and planning (0781), limited to~~
- 25 ~~5,000 square feet per floor.~~
- 26 ~~42. Large Appliance Repair Service (7623).~~
- 27 ~~43. Laundry and drycleaners agents, garment pressing, linen supply,~~
- 28 ~~cleaning services (7212, 7213, 7219); no coin-operated laundries~~
- 29 ~~or drycleaners.~~
- 30 ~~44. Legal services (8111).~~
- 31 ~~45. Libraries (8231).~~
- 32 ~~46. Management services (8741, 8742).~~
- 33 ~~47. Medical equipment rental and leasing (7352).~~
- 34 ~~48. Membership organizations - miscellaneous (8699).~~
- 35 ~~49. Museums and art galleries (8412).~~
- 36 ~~50. Musical instrument stores (5736).~~
- 37 ~~51. Outdoor advertising services (7312).~~
- 38 ~~52. Paint stores (5231).~~
- 39 ~~53. Parks, public or private; limited to pocket parks only, generally~~
- 40 ~~described as a small area accessible to the general public that often~~
- 41 ~~includes plantings, fountains, seating areas, and other similar~~
- 42 ~~passive open space features.~~
- 43 ~~54. Personal services - miscellaneous (7299, babysitting bureaus,~~
- 44 ~~clothing and costume rental, dating service, depilatory salons, diet~~
- 45 ~~workshops, dress suit rental, electrolysis, genealogical~~
- 46 ~~investigation service, and hair removal only).~~
- 47 ~~55. Personnel supply services (7363, except labor pools).~~

- 1 ~~56. Photocopying and duplicating services (7334).~~
- 2 ~~57. Photofinishing laboratories (7384).~~
- 3 ~~58. Photographic studios, portrait (7221).~~
- 4 ~~59. Physical fitness facilities (7991).~~
- 5 ~~60. Political organizations (8651).~~
- 6 ~~61. Professional membership organizations (8621).~~
- 7 ~~62. Public relations services (8743).~~
- 8 ~~63. Radio, television and consumer electronics stores (5731).~~
- 9 ~~64. Radio, television and publishers advertising representatives (7313).~~
- 10 ~~65. Record and prerecorded tape stores (5735).~~
- 11 ~~66. Real estate (6512, 6531, 6541).~~
- 12 ~~67. Retail - miscellaneous (5921-5963 and 5992-5999, excluding liquor~~
- 13 ~~stores, pawn shops, retail firearm and ammunition sales), limited to~~
- 14 ~~5,000 square feet per floor.~~
- 15 ~~68. Retail nurseries, lawn and garden supply stores (5261).~~
- 16 ~~69. Schools - vocational (8243-8299).~~
- 17 ~~70. Secretarial and court reporting services (7338).~~
- 18 ~~71. Security and commodity brokers, dealers, exchanges, and services~~
- 19 ~~(6211-6289).~~
- 20 ~~72. Shoe repair shops or shoeshine parlors (7251).~~
- 21 ~~73. Surveying services (8713), limited to 5,000 square feet per floor.~~
- 22 ~~74. Tax return preparation services (7291).~~
- 23 ~~75. United States Postal Service (4311, except major distribution~~
- 24 ~~center).~~
- 25 ~~76. Videotape Rental (7841), limited to 1,800 square feet of gross floor~~
- 26 ~~area.~~
- 27 ~~77. Wallpaper stores (5231).~~
- 28 ~~78. Watch, clock and jewelry repair (7631).~~

~~5. Accessory uses. Accessory uses within the GGDCCO include the uses listed below.~~

- ~~a. Caretaker's residence, accessory to commercial and mixed use projects only.~~
- ~~b. Enameling, painting, or plating, accessory to an artist's studio or craft studio only.~~
- ~~c. Play areas and playgrounds.~~
- ~~d. Recreational facilities.~~

~~6. Conditional uses. Conditional uses within the GGDCCO include the uses listed below, subject to the standards and procedures established in section 10.08.00.~~

- ~~a. Auctioneering Services, auction rooms and houses (5999, 7389); limited to 5,000 square feet per floor.~~

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- ~~b. Community centers.~~
- ~~c. Dance studios, schools, and halls (7911).~~
- ~~d. Food stores (5411-5499), over 5,000 square feet.~~
- ~~e. Motion picture theaters (7832).~~
- ~~f. Outdoor dining areas, not directly abutting the Golden Gate Parkway right-of-way.~~
- ~~7. Prohibited uses. Prohibited uses within the GGDCCO include the uses listed below:~~
 - ~~a. New residential-only structures.~~
 - ~~b. Any commercial use employing drive-up, drive-in or drive-through delivery of goods or services.~~
 - ~~c. Sexually oriented businesses (Code of Laws, 26-151 et seq.).~~
 - ~~d. Enameling, painting or plating as a primary use. However, these uses are permitted if secondary to an artist's or craft studio.~~
 - ~~e. Single-room occupancy hotels, prisons, detention facilities, halfway houses, soup kitchens or homeless shelters.~~

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2.05.01 - Density Standards and Housing Types

A. Where residential uses are allowable, the following **density** standards and housing type criteria shall apply.

Housing Type: Zoning District:	Single-family	Duplex	Townhouse	Multifamily	Mobile Home	Cluster	Guest House	Caretaker Units (number allowed)	Timeshare	Recreational vehicles ¹	Maximum Density ^{2,17} (units per gross acre)
*	*	*	*	*	*	*	*	*	*	*	*
GGDCCO <u>GGPOD</u>											Per <u>GGAMP</u> and/or the underlying zoning district

#

4.02.26 - Design Standards for the Golden Gate Parkway Overlay District (GGPOD).

DRAFT

Text underlined is new text to be added
Text strikethrough is current text to be deleted

A. General.

1. Conflict. In the event of a conflict between the provisions of the LDC and these regulations, the regulations contained in the GGPOD shall prevail.
2. Exemptions.
 - a. All development, including mixed use projects on properties zoned C-1, C-2, and C-3 in the GGPOD are exempt from the specific design criteria for mixed use development as prescribed in LDC section 4.02.38.
 - b. “Vertical mixed use” projects, as defined in LDC section 2.03.07 F.4., are exempt from the architectural and site design standards of LDC section 5.05.08., except for subsections F.4. (service function areas and facilities) and F.6. (drive-through facilities location and buffering standards), which shall still apply.
 - c. The wall requirement between nonresidential development and residentially-zoned properties pursuant to LDC section 5.03.02 H. is exempt for properties within the GGPOD.

B. Architectural, building, and site design standards.

1. Dimensional and design standards for the GGPOD. The standards contained herein are applicable to all developments within the GGPOD.

Table 1. Dimensional Requirements in the GGPOD.

	<u>Residential Use</u>	<u>Non-Residential Use and Mixed Use</u>
<u>Min. Lot Area (square ft)</u>	<u>Townhouse: 2,000</u> <u>Multi-Family:</u> <u>10,000</u>	<u>10,000</u>
<u>Min. Lot Width (linear ft)</u>	<u>Townhouse: 20</u> <u>Multi-Family: 100</u>	<u>100</u>
<u>Max. Floor Area Ratio</u>	<u>N/A</u>	<u>N/A</u>
<u>Max. Floor Area Ratio (hotels only)</u>	<u>N/A</u>	<u>0.90</u>
<u>Min. Floor Area (square ft per unit)</u>	<u>Efficiency: 450</u> <u>1 Bedroom: 600</u> <u>2+ Bedroom: 750</u>	<u>N/A</u>
<u>Max. Building Coverage (%)</u>	<u>100</u>	
<u>Required Yards¹</u>		
<u>Min. Front Yard (ft)</u>	<u>0</u>	
<u>Max. Front Yard (ft)</u>	<u>15²</u>	
<u>Min. Side Yard (ft)</u>	<u>0</u>	
<u>Min. Rear Yard (ft)³</u>	<u>0</u>	

<u>Max. Zoned Height (ft)</u>		
<u>GGPOD-AC</u>	<u>Townhouse: 45</u> <u>Multi-Family: 75</u>	<u>75</u>
<u>GGPOD-DT</u>	<u>Townhouse: 45</u> <u>Multi-Family: 60</u>	<u>60</u>

(Table to be added)

Notes:

¹ All structures shall comply with the safe sight distance triangles at intersection and access points of LDC section 4.06.01. D and the clear sight requirements of LDC section 6.06.05.

² For buildings set back greater than zero feet, the resultant yard must be designed to accommodate pedestrians, bicyclists, and convenient curb front access with a combination of planting areas, seating, extended sidewalk, or hardscaping. Balconies, awnings, rooves, cornices, eaves, gutters, fire escapes, stairways, sills, window or wall-mounted air-conditioning units, and other common building appurtenances and architectural features may protrude a maximum of six feet beyond the front property line. Where such protrusion occurs, a minimum vertical clearance of nine feet must be maintained between the underside of the overhang and the abutting sidewalk and such sidewalk must be at least six feet in width and clear of any obstruction. Foundation landscaping shall be required for parking structures and such structures shall be setback in accordance with LDC section 4.02.26 B.3.b.ii.

³ An alley will designate the rear yard of a lot.

2. All non-residential, mixed-use, and multi-family buildings. Unless otherwise stated, all development in the GGPOD shall comply with the following:

a. Buildings or projects that terminate a vista at the end or turning point of any street or pedestrian path shall include design features to emphasize the importance of the view. Examples of design features include but are not limited to tower elements, porte-cocheres, cupolas, etc.

b. Blank walls. Building wall areas must be interrupted with a minimum of an opening or relief work every 120 square feet.

c. Maximum uninterrupted building length. Where a building or series of buildings form a continuous front facade that exceeds 300 feet in length, a pedestrian pathway shall be provided on the ground level.

d. Roof material. Asphalt shingles are prohibited.

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- 1 e. Materials. Corrugated or metal panels are limited to no more than 33
- 2 percent of exterior building facades (not applicable to roofs).
- 3
- 4 f. Neon type tubing. Neon or neon type tubing and LED tubing is prohibited
- 5 if exposed on the building exterior, the roof of a building, or inside if within
- 6 window frames.
- 7
- 8 g. Architectural massing. Where property within the GGPOD is adjacent to
- 9 residential development external to the GGPOD or residentially zoned
- 10 property external to the GGPOD, transitional massing elements as
- 11 specified in 5.05.08 D.3. shall be provided on proposed buildings that are:
- 12
- 13 i. Twice the height or more of the adjacent residential building; or
- 14
- 15 ii. Twice or more of the allowable height in the adjacent residential
- 16 zoning district where no building exists or where there is a vacant
- 17 lot.
- 18
- 19 h. Parking structures. All parking structures within the GGPOD shall be
- 20 designed in accordance the following:
- 21
- 22 i. Parking structure façades shall be designed to screen views of
- 23 automobiles by the general public from adjacent streets and
- 24 driveways.
- 25
- 26 ii. Freestanding light fixtures on the top level parking structures shall
- 27 be a maximum of 20 feet in height. Lighting shall incorporate full-
- 28 cut-off fixtures to contain light to the surface of the deck only.
- 29
- 30 iii. Parking structures are allowed to be located below grade and below
- 31 habitable space if the stormwater criteria in the Code of Ordinances
- 32 90-41(f)(2) are met. These structures must be accessed from the
- 33 rear of the building.
- 34
- 35 3. Landscape. Landscaping shall be provided in accordance with the LDC section
- 36 4.06.00, except as provided herein.
- 37
- 38 a. Buffers. No landscape buffer is required along any property boundary on
- 39 a lot or parcel located within the GGPOD. However, nonresidential
- 40 developments proposed on lots or parcels abutting residentially-zoned
- 41 properties outside the GGPOD shall provide a minimum five-foot wide
- 42 landscape planting strip. The planting strip shall be landscaped with shrubs
- 43 and ground cover. Shrubs shall be planted in a double row and be no less
- 44 than 24 inches in height at time of planting. Trees shall be planted one per
- 45 30 linear feet. These landscape buffers may be interrupted to provide
- 46 pedestrian access only.
- 47

- 1 b. Foundation plantings. Foundation plantings for buildings and parking
- 2 structures shall be designed in accordance with LDC section 4.06.05,
- 3 except that the total size of the planting area shall be the equivalent to 10
- 4 percent of the ground floor area of the building. In addition, the following
- 5 provisions shall apply:
- 6
- 7 i. For buildings, including parking structures with ground floor retail or
- 8 residential, the foundation plantings shall be located within 25 feet
- 9 of the building edge in the form of landscaped courtyards and
- 10 seating area landscaping. However, the following design
- 11 considerations are exempt from the foundation planting
- 12 requirements:
- 13
- 14 a) In the front yard where a structure is designed in accordance
- 15 with the maximum setback requirement; or
- 16
- 17 b) Along the side property lines where buildings have been
- 18 constructed with a zero-foot setback.
- 19
- 20 ii. Stand-alone parking structures and those designed without ground
- 21 floor retail or residential uses along the front façade shall have a
- 22 minimum 10-foot wide building foundation planting pursuant to LDC
- 23 section 4.06.05 C. Where the parking structure is attached to the
- 24 building or adjacent to a preserve area, and the preserve area
- 25 meets the otherwise required landscaping, no additional
- 26 landscaping is required.
- 27
- 28 c. Off-street parking lot and vehicular use landscaping shall be designed in
- 29 accordance with LDC section 4.06.03, except in the following instances:
- 30
- 31 i. A maximum of 30 percent of the landscape islands shall have a
- 32 minimum of five feet inside planting area and may be planted with
- 33 a palm tree equivalent.
- 34
- 35 ii. The perimeter of all new parking lots fronting on public rights-of-way
- 36 shall be screened to a minimum height of two and one-half feet
- 37 using landscaping, streetwalls in accordance with LDC section
- 38 4.02.26 B.9.c.ii., fences, or any combination thereof.
- 39
- 40 iii. Parking lot perimeter landscaping areas shall be a minimum of eight
- 41 feet in width. Shrubs shall be arranged in a staggered pattern with
- 42 a minimum size of three gallons at the time of planting to provide
- 43 year-round screening. Trees shall be included in the perimeter
- 44 landscape area at a minimum spacing of one tree/palm per 25 feet
- 45 of linear frontage. Street trees planted within the right-of-way may
- 46 be used to meet this requirement.
- 47

- 1 iv. Plantings shall be a maximum of 25 percent turf grass. The balance
- 2 shall be groundcover, low shrubs and/or flowers located in planting
- 3 areas appropriate to the design.
- 4
- 5 4. Signage. The sign standards of LDC section 5.06.02 B.7. and LDC section 5.06.04
- 6 shall apply to all mixed use and non-residential projects within the GGPOD,
- 7 including non-residential uses on residentially-zoned properties. In addition, the
- 8 following provisions shall be available for all wall, awning, ground, blade, and menu
- 9 signs for all nonresidential and mixed use projects:
- 10
- 11 a. Awning signs. Signs on soft-sided fabric awnings are allowed to be
- 12 combined with wall signs, to the extent that the total combined square
- 13 footage does not exceed the maximum allowed for the wall sign.
- 14
- 15 b. Graphic elements, logos, mosaic tiles, or names created in flooring
- 16 immediately outside a tenant’s front door with a maximum size of six square
- 17 feet are allowed.
- 18
- 19 c. Plaques. Non-illuminated plaques are allowed for multi-tenant
- 20 developments and shall be provided for as follows:
- 21
- 22 i. Front door. Each tenant may mount a maximum of two plaques
- 23 at their storefront with each plaque restricted to a maximum of two
- 24 square feet. Plaques must be flush or pin-mounted on the
- 25 storefront or façade.
- 26
- 27 ii. Rear door. A plaque with a maximum size of two square feet
- 28 may be mounted at the rear door of each tenant. The purpose of
- 29 this plaque is to identify the business name or address for
- 30 emergency response or for the deliver of goods.
- 31
- 32 d. Under-canopy/blade sign. Each ground floor business in a multiple-
- 33 occupancy development is allowed one under-canopy blade sign in
- 34 conformance with LDC section 5.06.04 F.7 with the exception that the sign
- 35 area shall be a maximum of eight square feet. All signs, including the
- 36 structural supports to which each sign will be affixed, shall comply with the
- 37 weight load requirements in accordance with the FBC.
- 38
- 39 e. Menu board. A wall sign not to exceed three square feet is allowed in
- 40 connection with a walk-up or take-out window. This sign shall not count
- 41 toward the maximum size allowed for wall signs.
- 42
- 43 5. Service areas. All project types, including vertical mixed use, shall be subject to
- 44 LDC section 5.05.08 F.4. In addition, for projects within the GGPOD, the loading
- 45 docks, solid waste facilities, recycling facilities, and other services elements shall
- 46 be placed to the rear or side yard of the building in visually unobtrusive locations

1 with minimum impacts on view. Businesses are encouraged to consolidate and
2 share refuse areas and equipment.

3
4 6. Public open space.

5
6 a. In order to promote a vibrant, walkable, urban community, the GGPOD
7 public open space standards are intended to concentrate open space in
8 structured, functional, and usable spaces that are nodes along connected
9 pedestrian paths or corridors that are accessible to the public.

10
11 b. A minimum of 15 percent of the site shall be dedicated to structured open
12 spaces that are accessible to the public along pedestrian paths or corridors.
13 The open spaces may be pocket parks, green space, pervious plazas,
14 central greens, and may include passive furnishings such as benches,
15 pavilions, picnic areas, and similar furnishings. Pervious trail links that
16 connect within or across open spaces may count toward the requirement.
17 Lakes, detention areas with publicly accessible sidewalks or other
18 impervious paths, and active recreation fields or courts that are accessible
19 to the public may be counted toward the required public open space.
20 Provisions shall be included to assure the continued maintenance of the
21 public open space area.

22
23 7. Enhanced internal sidewalks. Enhanced internal sidewalks are required pursuant
24 to LDC section 5.05.08 F.3., except that sidewalks shall be six feet in width.

25
26 8. Exterior lighting.

27
28 a. Public paths shall be clearly marked by using design elements such as
29 landscaping and pedestrian accent lighting.

30
31 b. Lighting fixtures within off-street parking lots shall be a maximum of 25 feet
32 in height.

33
34 c. Illumination levels shall not exceed 0.5 footcandles at lot lines that are
35 adjacent to residential development external to the GGPOD or
36 residentially-zoned property external to the GGPOD. Lighting fixtures must
37 have full-cutoff optics that direct the light source downward.

38
39 9. Type A Frontage. Unless otherwise specified, the standards contained herein are
40 applicable to all properties fronting on Primary Streets, which are depicted in the
41 Regulation Plans in LDC section 2.03.07 F.6.

42
43 a. Intersections. Buildings or projects located at the intersection of two or
44 more arterial or collector roads, or two or more Primary Streets, shall
45 include design features to emphasize their location as gateways and
46 transition points within the community. Examples of required design
47 features include: tower element, public plaza, or courtyard.

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b. Required building design along Type A Frontage.

- i. First story. Buildings shall be designed with a minimum finished floor to ceiling height of 12 feet.
- ii. Second and higher stories. The second and higher stories shall be set back from the first story façade by at least 10 feet. This set back requirement is not applicable to the Type A Frontage requirement along Golden Gate Parkway.

Illustration 2. Minimum required dimensions.

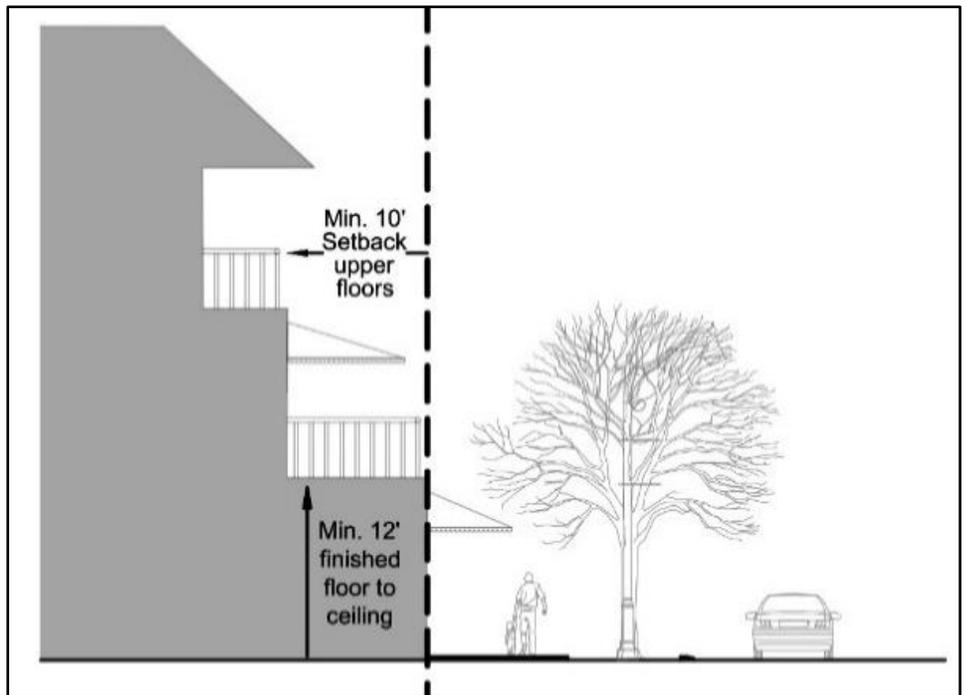


Image by Jeffrey Michael George

(Image to be added)

14

- iii. Windows. The ground floor of non-residential buildings shall have at least 60 percent of its façade designed with windows to provide visual interest for pedestrians and to serve as a deterrent to crime.

c. Off-street parking, vehicular use, and service area standards.

- i. Vehicular use, parking and service areas shall be located to the rear or sides of buildings. When located to the sides of buildings, vehicular use areas shall occupy less than 50 percent of the length

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1 of the Type A Frontage line to maintain a degree of pedestrian scale
2 and comfort along the street front.

3
4 ii. Vehicular use areas shall be screened from Type A Frontages,
5 public paths and pedestrian corridors, and residential-only
6 development, including those external to the GGPOD, with either a
7 two and one-half foot streetwall or a row of hedges or any
8 combination of the two. If hedges are used, they shall be spaced
9 no greater than three feet on-center, except where such vehicular
10 use areas are obscured from view by an intervening building or
11 where such vehicular use area is in connection with a driveway of
12 a townhouse. If a streetwall is used, it shall be designed in
13 accordance with the following:

14
15 a) A minimum of two and one-half feet in height and made of a
16 material matching the adjacent building.

17
18 b) Openings shall be spaced no greater than 25 feet in width
19 to allow automobile and pedestrian access or to
20 accommodate a plaza or public space.

21
22 iii. Along Type A Frontage, parking structures shall have commercial
23 or residential uses occupy the ground floors.

24
25 iv. Vehicular use areas that are not obscured from view by a building,
26 except for townhouse driveways, shall be screened from view when
27 located along Type A Frontage, a public path or public pedestrian
28 corridor, or a residential development or a residentially zoned
29 property external to the GGPOD, with a wall, continuous hedge or
30 other durable nonvegetative barrier, or combination thereof,
31 measuring three to four feet in height, along the entire length of the
32 vehicular use area, except within sight triangles.

33
34 10. Required public transit facilities. The standards contained herein are applicable to
35 all projects in the GGPOD having more than 50 dwelling units or a unified
36 development with at least 10,000 square feet of commercial uses, industrial uses,
37 or a combination of commercial and industrial uses.

38
39 a. A paved sidewalk to the nearest bus stop shall be provided if the bus stop
40 or other transit facility is within 330 feet from the primary building entrance,
41 the developer must install signage, a landing pad, and bicycle storage rack
42 within the road right-of-way or dedicated easement if the bus stop is not so
43 equipped.

44
45 b. If there is no bus stop within 330 feet from the primary building entrance
46 and the property abuts the bus route, the developer shall provide signage,

1 a bicycle storage rack, and landing pad for a new bus stop within the road
2 right-of-way or dedicated easement.

3
4 11. Design of Primary Streets and Secondary Streets (alleys). The standards
5 contained herein are applicable to all new primary streets and secondary streets
6 (alleys) in the GGPOD.

7
8 a. Primary Streets (excluding Golden Gate Parkway). Primary Streets are to
9 form a modified grid to provide continuous pedestrian walkways. The
10 Primary Street network corresponds to a mix of uses and a higher level of
11 site and building design standards that are intended to promote pedestrian
12 comfort and foster a vibrant urban realm. The frontage condition for
13 property along Primary Streets is subject to Type A Frontage standards of
14 LDC section 4.02.26 B.9. Primary Streets shall be designed with
15 sidewalks, street tree areas, parallel on-street parking areas with curbing
16 and travel lanes for vehicles and cyclists. In addition, the following shall
17 apply:

18
19 i. Public infrastructure, including but not limited to stormwater,
20 potable water, and sanitary sewer may be placed underground
21 within the areas designated for sidewalks, on-street parking, or
22 travel lanes.

23
24 ii. Lighting fixtures shall be provided along Primary Streets consistent
25 with the decorative lighting identified in the Golden Gate Community
26 Roadways Beautification Master Plan. If such lighting becomes
27 unavailable, similarly themed lighting shall be used.

28
29 iii. Seating for outdoor dining shall be permitted to encroach onto the
30 public sidewalks with the right-of-way, provided that a minimum of
31 five feet of clear pedestrian passage is maintained on the sidewalk,
32 between the outdoor dining and the right-of-way street tree
33 landscaped area, paved roadway, or vehicular travel lane,
34 whichever is applicable and the most restrictive.

35
36 b. Secondary Streets (alleys). Secondary Streets are to function as
37 secondary accessways or alleys. The Secondary Streets are meant to
38 maintain necessary vehicle and service access, including access to parking
39 lots and parking structures. Secondary Streets shall be a minimum of 20
40 feet in width. Where property is adjacent to residential development
41 external to the GGPOD or residentially zoned property external to the
42 GGPOD, vehicular ingress and egress shall be located along Secondary
43 Streets or alleys and located to avoid conflict with traffic along streets
44 external to the GGPOD.

45
46 12. Perimeter walls. The masonry, concrete, or pre-fabricated concrete wall
47 and/or fence requirement between nonresidential development and residentially-

1 zoned properties pursuant to LDC section 5.03.02 H. is not applicable in the
2 GGPOD.

3
4 C. Off-street parking requirements. The standards contained herein are applicable to all
5 developments in the GGPOD.

6
7 1. Required off-street parking.

8
9 a. Non-residential. The minimum number of off-street parking spaces for non-
10 residential uses shall be required as follows:

11
12 i. Hotels shall provide one space per hotel room.

13
14 ii. Food truck parks shall provide parking as follows:

15
16 a) Restaurant/Food truck pads/restrooms/storage: one space
17 per 150 square feet;

18
19 b) Bar area: one space per 50 square feet; and

20
21 c) Outdoor or covered seating area: one space per 75 square
22 feet.

23
24 iii. All other non-residential uses shall provide in accordance with LDC
25 section 4.05.04 but not to exceed three spaces per 1,000 square feet gross
26 floor area.

27
28 b. Residential:

29
30 i. Required number of parking spaces for residential units shall be in
31 accordance with LDC section 4.05.04.

32
33 ii. Spaces required for residential units must be dedicated in an on-
34 site parking structure, garage, townhouse driveway, or off-street
35 parking space(s), with the exception that a dedicated on-street
36 parking space developed as part of a Primary Street and located
37 within 330 feet measured from the property line may substitute for
38 one on-site parking space.

39
40 2. Reductions. Off-street parking requirements of LDC section 4.02.26 C. may be
41 reduced cumulatively by the following provisions, in any combination:

42
43 a. Excluding food truck parks, no additional off-street parking is required for
44 outdoor dining or outdoor restaurant seating areas.

45
46 b. For mixed use projects, the required number of residential off-street parking
47 spaces may be reduced by 10 percent.

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- c. If the subject property is located within the GGPOD-AC and within 330 feet of an improved public transit facility, such as a bus shelter, bus transfer facility, or park and ride site, a 20 percent reduction in required off-street parking spaces is permissible.
- d. If the subject property is located within the GGPOD-DT and within 330 feet of an improved public transit facility, such as a bus shelter, bus transfer facility, or park and ride site, a 10% reduction in required off-street parking spaces is permissible.
- e. The County Manager or designee may approve valet parking, subject to an SDP or SDPA demonstrating the operation, including traffic levels and hours of operation, will not adversely impact surrounding properties and residential neighborhoods. Establishments providing valet parking services may not utilize parking areas designated for shared use for the storage of vehicles parked by this service, unless allowed by a shared parking agreement
- f. For mixed use projects, residential areas that are within 0.125 mile of a commercial area but are not directly accessible by a vehicle due to gating or lack of vehicular interconnection may not utilize on-street parking in the commercial area to meet the residential parking requirement.
- g. Residential areas may utilize on-street parking that is abutting a residential unit to meet the parking requirement in a one to one (1:1) ratio. If parking spaces are used to meet a residential parking requirement, they may not then be utilized to meet any of the commercial requirement.
- h. Parking credits. The County Manager or designee may grant parking credits to reduce the minimum parking requirement for non-residential and mixed uses for any combination of the following development strategies:
- a) Each space dedicated for parking or charging of electric vehicles or scooters may be substituted for standard required parking spaces up to a maximum quantity of 10 percent required parking spaces, but not to exceed a maximum of 20 substituted parking spaces.
 - b) Compact vehicle spaces measuring eight feet in width by 16 feet in length may substitute for standard required parking spaces up to a maximum quantity of 10 percent of the required parking spaces.
 - c) If a public transit facility, such as bus shelter, bus transfer facility, or park and ride accommodation, is provided on-site and in coordination with and accepted by the Collier Area Transit, or if a bike-share station or ride-share pick up/drop off zone is provided on-site, the facility may substitute for a portion of the standard

1 required off-street parking spaces and may combine parking space
2 reduction percentages with LDC section 4.02.26 C.2.b and LDC
3 section 4.02.26 C.2.c. The total percentage of parking reduction
4 shall only equal a maximum of 30 percent of the minimum required
5 off-street parking spaces. The County Manager or designee may
6 approve an alternative parking credit if the applicant provides a site-
7 specific analysis of the projected frequency of passenger or patron
8 use demonstrating a greater off-set of parking spaces is warranted.
9

10 i. The minimum number of spaces required may be adjusted by the County
11 Manager or designee when it has been determined that the reduction is
12 necessary to preserve a healthy tree or trees (with a 12-inch or greater
13 diameter at breast height) from being damaged or removed, and where the
14 SDP provides for the retention of said tree or trees.
15

16 D. Design standards for Employment uses in the GGPOD. The following performance
17 standards shall be applicable to all Employment uses identified in Table 1 of LDC section
18 2.03.07 F.6.:
19

20 1. Loading areas. All loading areas shall be oriented away from adjacent
21 residential uses, except for where obstructed by an intervening building.
22

23 2. Outside storage and display. No outside storage and display shall be permitted
24 except for which may be approved as part of a temporary/special event in
25 accordance with LDC section 5.04.05.
26

27 3. Operations. All uses within this category, including but not limited to
28 manufacturing, process, and packing, shall be conducted within a fully enclosed
29 building. All manufacturing operations and equipment, including accessory
30 process equipment, such as compressors and air handlers, shall be contained in
31 an enclosed structure.
32

33 4. Noise. No use shall produce noise exceeding the sound level limits for
34 Commercial or Tourist uses as set forth in Ordinance No. 90-17, the Noise
35 Ordinance, as amended.
36

37 5. Objectionable odors. No person shall cause, suffer, allow, or commit the discharge
38 of air pollutants which contribute to an objectionable odor. No business shall
39 cause or allow the emission of odorous air from any single source, such as to result
40 in odors which are detectable outside the parcel boundaries. Best practical
41 treatment, maintenance, and control currently available shall be utilized in order to
42 maintain the lowest possible emission of odorous air.
43

44 6. Lighting. Unless obstructed by an intervening building, lighting shall be
45 located so that no light is aimed directly toward a property with existing residential
46 uses, residential zoning, or otherwise designated residential.
47

1 7. Vibrations. No use shall operate to produce ground vibration noticeable by a
2 person of reasonable sensitivity at the property line.

3
4 8. Smoke and Particulate Matter. Toxic or noxious matter. No use shall for any
5 period of time, discharge across the boundaries of a lot on which it is located, toxic
6 or noxious matter in such concentrations as to be detrimental to or endanger the
7 public health, safety, comfort, or general welfare, or cause injury or damage to
8 persons, land, or the use of land, or render unclean the waters of the state to the
9 extent of being harmful or inimical to the public health, or to animal or aquatic life,
10 or to the use of such waters for domestic water supply, industrial purposes,
11 recreation, or other legitimate and necessary uses.

12
13 9. No use shall include the storage of large volumes of highly flammable, toxic matter
14 or explosive materials as a primary use.

15
16 E. Deviation process for projects with site plans in the GGPOD.

17
18 1. General. Excluding maximum building height, deviation requests may be
19 made from specific dimensional or design requirements as described in this
20 section. A deviation request may be reviewed administratively or by the Office of
21 the Hearing Examiner. This section addresses the permissible deviations,
22 limitations thereon, and the review process. Property owners shall be eligible to
23 seek a deviation from the dimensional requirements of the following Code
24 provisions, unless otherwise noted.

25
26 a. LDC section 4.02.26. All dimensional standards of this section, excluding
27 building height, may be considered for a deviation request. In addition,
28 deviations from non-dimensional provisions are also allowed as substantial
29 deviations.

30
31 b. LDC section 4.05.06 B., utilizing the existing administrative deviation
32 process set forth in LDC section 4.05.04 F.4., recognizing that the reduced
33 need for off-street parking in the GGPOD may be offered as a viable basis
34 for such administrative deviation.

35
36 c. LDC section 4.06.03 B.

37
38 d. LDC section 4.06.05 B.3.

39
40 e. LDC section 4.06.05 C., including Table inset.

41
42 2. Review process. Insubstantial deviations will be reviewed administratively by
43 the County Manager or designee. Substantial deviations will be reviewed by the
44 Hearing Examiner. This section is not intended to replace the current established
45 process of requesting deviations through the PUD rezoning process. Any
46 deviations from the LDC which are not expressly provided for in this section shall
47 be processed as variances in accordance with LDC section 9.04.00.

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3. Concurrent deviation application required. All deviation requests shall be made concurrently with an application for an SDP, SDPA, or SIP or in the case of sign deviations, with a building permit. The applicant shall list all requested deviations on the required site plan(s) and depict the deviation(s) graphically on the plan(s). Additional graphic information may also be required by staff, on a case-by case basis.
4. Insubstantial deviations. Excluding maximum building height, requested deviations that do not exceed 10 percent of the required dimension, amount, size, or other applicable dimensional standard, except for the required number of parking spaces, which may not exceed 20 percent of the LDC requirement (not more than 10 spaces), are insubstantial. To be approved, the following criteria must be considered:
- a. The proposed deviation is compatible with adjacent land uses and structures, achieves the requirements of the regulations as closely as is practicable, and meets the intent of the related LDC regulations; and
 - b. The applicant proposes equitable tradeoffs for the proposed deviation of development standards, such as increased open space, landscaping, pedestrian spaces, buffering or architectural features, in order to meet the intent of the regulation being diminished.
5. Substantial deviations. Requested deviations that do not qualify as insubstantial deviations are substantial deviations. When evaluating a substantial deviation, the Hearing Examiner shall consider the following:
- a. Whether or not the proposed deviation is compatible with adjacent land uses and achieves the requirements and/or intent of the regulations as closely as is practicable;
 - b. Whether the proposed deviation is the minimum amount necessary to allow for reasonable use of the property and/or address the issue necessitating the deviation request; and
 - c. Whether the reduced or increased standard requested by the deviation is mitigated for, either on the subject site or by providing a public benefit on the subject site. Examples of such on-site mitigation include but are not limited to: increasing setbacks from the adjacent road right-of-way when proposing to deviate from sign size limitations; increasing plantings or planting sizes or installing a fence or wall where a reduced buffer width is proposed; providing public pedestrian and/or bicycle pathway easements or other similar mobility improvements including transit enhancements; providing public parking; providing beautification in the public realm, including street trees, street furniture, lighting and other similar public benefits.

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6. Public notice. Public notice, including signage, notice to property owners, and an advertised public hearing, is required for substantial deviation requests and shall be provided in accordance with the applicable provisions of LDC section 10.03.06 F.

#

~~4.02.37 – Reserved. Design Standards for Development in the Golden Gate Downtown Center Commercial Overlay District (GGDCCO)~~

~~A. Development criteria. The following standards shall apply to all uses in this overlay district. Where specific development criteria and standards also exist in the Golden Gate Area Master Plan, or the Future Land Use Element of the Growth Management Plan, these standards shall supersede any less stringent requirement or place additional requirements on development.~~

~~1. Cessation of residential uses. Existing, non-owner-occupied residential uses located along Golden Gate Parkway shall cease to exist no later than seven (7) years after the effective date of the adoption of the Downtown Center Commercial Subdistrict in the Golden Gate Area Master Plan (adopted October 26, 2004; effective January 14, 2005). This does not require the removal of residential structures located on Golden Gate Parkway that are converted to uses allowed in this overlay district within one (1) additional year; nor does this require the removal of residential structures located elsewhere in this overlay district.~~

~~2. Multi-story buildings.~~

~~a. Only retail, personal service, and institutional uses are allowed on the first floor.~~

~~b. All uses allowed by this zoning overlay, except restaurants and cocktail lounges, are allowed on the second floor.~~

~~c. Only residential uses are allowed on the third floor.~~

~~3. Density. Density shall be as per the underlying zoning district. For mixed-use projects, density shall be calculated based upon total project acreage.~~

~~4. Setbacks.~~

~~a. All development and redevelopment on lots abutting Golden Gate Parkway shall have a front yard and setback of no more than 15 feet. All projects providing a front yard setback greater than 0 feet must provide restaurant seating, and/or open space areas such as other seating, planting areas, and decorative landscape planters within such front yard, except that water management retention and detention areas are prohibited.~~

- 1 ~~b. All development and redevelopment on properties in the overlay area not~~
- 2 ~~abutting Golden Gate Parkway must comply with the front yard setback~~
- 3 ~~requirements of that property's underlying zoning.~~
- 4
- 5 ~~c. Side yard setback shall be a minimum of 5 feet.~~
- 6
- 7 ~~d. Rear yard setback shall be no less than 1/2 of the building height, with a 15-~~
- 8 ~~foot minimum.~~
- 9
- 10 ~~5. Building footprint limits.~~
- 11
- 12 ~~a. Minimum building footprint of 3,000 square feet.~~
- 13
- 14 ~~b. Maximum building footprint of 12,000 square feet.~~
- 15
- 16 ~~6. Minimum floor area—Residential.~~
- 17
- 18 ~~a. Mixed use. Minimum floor area of 1,500 square feet.~~
- 19
- 20 ~~7. Minimum height. The zoned height of buildings shall be no less than 34 feet.~~
- 21
- 22 ~~8. Maximum height. The actual height of buildings shall not exceed 3 stories or 45~~
- 23 ~~feet.~~
- 24
- 25 ~~9. Crime Prevention Through Environmental Design (CPTED) Standards. The~~
- 26 ~~Golden Gate community supports the CPTED philosophy as a way to reduce~~
- 27 ~~crime, improve neighborhood and business environments, and increase overall~~
- 28 ~~quality of life of its citizens. CPTED principles such as natural surveillance, natural~~
- 29 ~~access control, and territorial reinforcement shall be incorporated into the overall~~
- 30 ~~design of the project, consistent with the provisions in this overlay and as identified~~
- 31 ~~below.~~
- 32
- 33 ~~a. Public paths shall be clearly marked by using design elements such as~~
- 34 ~~landscaping and pedestrian accent lighting.~~
- 35
- 36 ~~b. Public entrances shall be clearly defined by walkways and signage, as~~
- 37 ~~specifically provided in this overlay.~~
- 38
- 39 ~~10. Common architectural style. In support of the purpose and intent of the GGDCCO,~~
- 40 ~~all structures within the overlay district shall have a common Mediterranean~~
- 41 ~~architectural style, with barrel tile roofs, stucco façades, arches and wood accent~~
- 42 ~~members used as details. During the site development plan review process,~~
- 43 ~~architectural drawings shall be submitted to demonstrate adherence to this~~
- 44 ~~requirement. All commercial and mixed use buildings and projects shall be subject~~
- 45 ~~to the provisions of section 5.05.08 of the Code, except as provided herein.~~
- 46

- 1 ~~11. Architectural standards. All buildings shall meet the requirements set forth in~~
- 2 ~~section 5.05.08, except as otherwise specified below:~~
- 3
- 4 ~~a. Buildings with frontage on Golden Gate Parkway shall have 60 percent of~~
- 5 ~~the ground floor façade finished with clear or lightly tinted glass.~~
- 6
- 7 ~~b. The glazed area of the façade above the first floor shall be at least twenty~~
- 8 ~~(20) percent but shall not exceed 35 percent of the total area, with each~~
- 9 ~~façade being calculated independently.~~
- 10
- 11 ~~c. Design elements used to embellish the primary façade shall be similarly~~
- 12 ~~incorporated into the rear façade of buildings.~~
- 13
- 14 ~~d. The façades of buildings 5,000 square feet or larger shall use rooflines that~~
- 15 ~~vary in height or architectural embellishments, such as cupolas, at least~~
- 16 ~~every 80 feet.~~
- 17
- 18 ~~e. Parapet roof treatments are prohibited.~~
- 19
- 20 ~~f. Flat roofs must be screened with a mansard edge barrel tile roof extending~~
- 21 ~~the length of all façades.~~
- 22
- 23 ~~g. Building entrances shall be accentuated through architectural elements,~~
- 24 ~~lighting, landscaping, and/or paving stones.~~
- 25
- 26 ~~12. Outdoor display/sale of merchandise.~~
- 27
- 28 ~~a. No automatic food and drink vending machines shall be located exterior to~~
- 29 ~~buildings.~~
- 30
- 31 ~~13. Access. Those aggregated lots developed under these overlay provisions and~~
- 32 ~~fronting on Golden Gate Parkway must provide site access from abutting local~~
- 33 ~~streets and may not obtain site access from Golden Gate Parkway, and where one~~
- 34 ~~such commercial or mixed use development provides site access at the project~~
- 35 ~~boundary abutting other lots, whether aggregated or not for commercial or mixed~~
- 36 ~~use development. Such developments must provide for joint or cross access with~~
- 37 ~~the abutting property or properties in order to share access and minimize the~~
- 38 ~~number of points from Golden Gate Parkway.~~
- 39
- 40 ~~14. Parking standards. All commercial and mixed use projects shall meet the parking~~
- 41 ~~requirements as set forth below:~~
- 42
- 43 ~~a. A minimum of 3 public parking spaces for each 1,000 square feet of~~
- 44 ~~commercial floor area.~~
- 45
- 46 ~~b. A minimum of 1.5 parking spaces for each residential unit.~~
- 47

- 1 ~~c. No parking is allowed in the front yard of lots abutting Golden Gate~~
- 2 ~~Parkway.~~
- 3
- 4 ~~d. There shall be no parking requirement for outdoor restaurant seating areas.~~
- 5
- 6 ~~e. Shared parking is required, where possible and feasible.~~
- 7
- 8 ~~f. Interconnection between adjacent parking lots is required, where possible~~
- 9 ~~and feasible.~~
- 10
- 11 ~~g. Parking facility lighting shall be consistent with the pedestrian/accent~~
- 12 ~~decorative lighting fixtures illustrated in Fig. 29, page 5-9 of the Golden~~
- 13 ~~Gate Community Roadways Beautification Master Plan, as amended, and~~
- 14 ~~shall be maintained at a light level of 3.0 foot candles, and arranged and~~
- 15 ~~shielded in a manner that protects roadways and neighboring properties~~
- 16 ~~from direct glare or other interference.~~
- 17
- 18 ~~h. All projects that are adjacent to residential development within the district~~
- 19 ~~and/or residentially zoned properties outside the district shall provide~~
- 20 ~~lighting fixtures with full-cutoff optics that direct the light source downward.~~
- 21
- 22 ~~i. The overnight parking of commercial vehicles, with a rated load capacity of~~
- 23 ~~one ton or more, is prohibited.~~
- 24
- 25 ~~j. The overnight parking of commercial vehicles shall be limited to 1 vehicle~~
- 26 ~~per 1,000 square feet of commercial floor area, not to exceed 6 commercial~~
- 27 ~~vehicles per building.~~
- 28
- 29 ~~15. Project standards. In addition to the site design elements described in section~~
- 30 ~~5.05.08 F.1, all projects shall provide:~~
- 31
- 32 ~~a. Two accept or specimen trees, above the minimum landscape code~~
- 33 ~~requirements, for every 100 linear feet along both the front and rear~~
- 34 ~~façades, at a minimum height of 18 feet at planting, except that projects~~
- 35 ~~with frontage along Golden Gate Parkway shall only be required to provide~~
- 36 ~~the planting along the rear façade.~~
- 37
- 38 ~~b. Decorative landscape planters or planting areas, a minimum of 5 feet wide,~~
- 39 ~~and areas for shaded seating consisting of a minimum of 100 square feet.~~
- 40
- 41 ~~16. Landscaping. All commercial and mixed use projects shall meet the landscape~~
- 42 ~~requirements in section 4.06.00 of the Code, unless otherwise specified in this~~
- 43 ~~zoning overlay.~~
- 44
- 45 ~~a. Project Perimeter Buffering. All projects that are located adjacent to~~
- 46 ~~residential zoning external to the district boundaries shall provide a 6-foot~~
- 47 ~~wide sidewalk and a minimum 19-foot wide landscape planting area, except~~

1 ~~that the project's frontage along Golden Gate Parkway shall be exempt~~
2 ~~from this requirement.~~

3
4 ~~The planting area shall be landscaped with: 1. shrubs and ground cover;~~
5 ~~shrubs shall be planted in a double row and be no less than 24 inches in~~
6 ~~height at time of planting; and, 2. Trees, planted one per 30 linear feet and,~~
7 ~~at time of planting, shall be a minimum height of 22 feet.~~

8
9 ~~b. Internal Project Buffering. All projects that are located adjacent to a~~
10 ~~residential use within the district boundaries shall provide an Alternative B~~
11 ~~buffer per section 4.06.02 C.2. except: walls and berms are prohibited, a~~
12 ~~freestanding hedge must be 6 feet in height, if a fence is provided it must~~
13 ~~be accompanied by a hedge 3 feet in height with a 2 foot spread at time of~~
14 ~~planting. The project frontage along Golden Gate Parkway shall be exempt~~
15 ~~from this requirement.~~

16
17 ~~c. Building Foundation Planting Areas. Building foundation plantings shall be~~
18 ~~required for all projects, except for buildings adjacent to Golden Gate~~
19 ~~Parkway and the rights-of-way abutting the district's external boundaries.~~

20
21 ~~d. Project Vehicular Use Areas. Mountable curbs shall be provided for all~~
22 ~~terminal landscape islands as depicted in Figure 3, Section 4.06.03.B.3.~~

23
24 ~~17. Golden Gate Parkway Right-of-Way Improvements. Right-of-way improvements~~
25 ~~required for properties/lots with frontage along Golden Gate Parkway.~~

26
27 ~~a. Curbing. All projects/lots shall provide Type "F" non-mountable curbing per~~
28 ~~F.D.O.T. "Roadway and Traffic Design Standards" Index 300, the latest~~
29 ~~edition.~~

30
31 ~~b. Landscaping. All projects/ lots shall be required to provide an 8 foot wide~~
32 ~~landscaping strip between the curb and sidewalk. This planting area may~~
33 ~~be reduced between tree spacing to 5 feet wide, for a maximum of 50~~
34 ~~percent of the buffer area, to accommodate street furnishings and fixtures,~~
35 ~~consistent with the Golden Gate Community Roadways Beautification~~
36 ~~Master Plan.~~

37
38 ~~The landscaping strip shall be landscaped with: 1. Turf, not to exceed 50~~
39 ~~percent of the planting area; 2. Shrubs and ground cover, at a minimum of~~
40 ~~50 percent coverage, not to exceed a mature height of 24 inches; and, 3.~~
41 ~~Canopy trees, planted one per 30 linear feet and, shall be at time of~~
42 ~~planting, a minimum 4-inch caliper with 8 feet of clear trunk and 22 feet in~~
43 ~~height.~~

44
45 ~~All planting materials in the public right-of-way shall be consistent with~~
46 ~~those identified in the Golden Gate Community Roadways Beautification~~
47 ~~Master Plan. Installation and maintenance shall be consistent with the~~

1 ~~Collier County Construction Standards Handbook for Work Within the~~
2 ~~Right-of-Way.~~

3
4 ~~Where right-of-way plantings above are not practicable, a planting area~~
5 ~~and/or a decorative planter(s) may be provided consistent with the Collier~~
6 ~~County Construction Standards Handbook for Work Within the Right-of-~~
7 ~~Way.~~

8
9 ~~Property owners shall be required to enter into a Landscape Maintenance~~
10 ~~Agreement with the County for the installation and maintenance of the~~
11 ~~required right-of-way plantings. Landscape Maintenance Agreements shall~~
12 ~~require a signed and sealed landscape and irrigation plan(s), review by the~~
13 ~~appropriate Transportation Division staff, approval by the Board of County~~
14 ~~Commissioners, and recording of said agreement with the Clerk of Courts.~~
15 ~~Plans shall include, but not be limited to, the following:~~

- 16
17 ~~i. Existing conditions inventory.~~
- 18
19 ~~ii. Proposed plantings/details of planting methods and~~
20 ~~maintenance specifications.~~
- 21
22 ~~iii. Location of utilities.~~
- 23
24 ~~iv. Location of drainage facilities.~~
- 25
26 ~~v. Irrigation proposal, including water connections.~~

27
28 ~~c. Sidewalks. All projects/lots shall provide a ten foot wide sidewalk between~~
29 ~~the required landscape strip identified in "b." above and the property line.~~
30 ~~Sidewalks shall be constructed pursuant to the Land Development Code~~
31 ~~and the "Construction Standards Handbook for Work Within the Right-of-~~
32 ~~Way". Decorative pavers (consistent with the approved pavers identified in~~
33 ~~the Golden Gate Community Roadways Beautification Master Plan) may~~
34 ~~be substituted for portions of sidewalk, subject to approval by the County~~
35 ~~Manager, or designee.~~

36
37 ~~Where a portion of the required sidewalk cannot be constructed in the~~
38 ~~public right-of-way, the property owner shall locate such portion on their~~
39 ~~private property and grant the County an easement.~~

40
41 ~~18. Address numbers. Address numbers shall be 8 inches in vertical height and shall~~
42 ~~be located on the primary building façade. Numbering materials shall be reflective~~
43 ~~and have a contrasting background.~~

44
45 ~~19. Signage. As required, allowed, or prohibited in section 5.06.00 of the Code.~~
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~~20. Lighting. As described and provided in the Golden Gate Community Roadways Beautification Master Plan or as identified below:~~

~~a. Internal Project Lighting. All projects shall use architectural decorative lighting. Such lighting shall be the same decorative lighting as identified in Figure 29 on page 5-9 of the Golden Gate Community Roadways Beautification Master Plan. If such lighting becomes unavailable, similarly themed lighting shall be used. Light fixtures must light all public use areas adjacent to the building (e.g. entryway, courtyards, etc.) to a recommended 0.5 candle level of illumination. Lighting shall be arranged in a manner that protects roadways and neighboring properties from direct glare or other interference.~~

~~b. Street/Roadway Lighting. Architectural decorative lighting shall be used along Golden Gate Parkway within the public right-of-way. Such lighting shall be a variation of the decorative lighting identified as Figure 29 on page 5-9 of the Golden Gate Community Roadways Beautification Master Plan, more specifically identified as the Lumec Domus Series (DMS50-250MH-SG3-480-LD-DL-CRL72-1A/U.S. 41 East lighting fixture). If such lighting becomes unavailable, similarly themed lighting shall be used. The installation and maintenance of lighting shall be consistent with the Collier County Construction Standards Handbook for Work Within the Right-of-Way.~~

~~21. Dumpsters. Dumpsters shall be screened and positioned out of view from public rights-of-way and pedestrian walkways.~~

#

4.02.038 - Specific Design Criteria for Mixed Use Development within C-1 through C-3 Zoning Districts

* * * * *

B. *Applicability.* All properties zoned C-1, C-2 and C-3, excluding where located in the GGPOD. These regulations shall apply to all mixed-use projects proposed within these zoning districts, subject to the design criteria set forth in this section. The design criteria address the relationship of **buildings**, parking, vehicular, and pedestrian movement to create a pedestrian oriented experience. **Buildings** are encouraged to be built close to the vehicular and pedestrian way to create a continuous active and vibrant **streetscape** utilizing the architecture, landscaping, lighting, signage, and **street** furnishings. Vehicular travelways support two-way traffic and on **street** parking. A logical pedestrian **pathway** system is provided throughout that connects the pedestrian movements from one use to another or within use areas. **Building** arcades and awnings are allowed to extend over the **sidewalk** to create shade and encourage pedestrian activity. Signage design shall be carefully integrated with site and **building** design to create a unified appearance for the project. Creativity in the design of **signs** is

encouraged in order to emphasize the unique character of the project. Projects utilizing these design criteria will be developed in compliance with the LDC, except as specified herein.

C. *Commercial Mixed Use Design Criteria.* Projects utilizing the Commercial Mixed Use option within a C-1, C-2, or C-3 Zoning District shall comply with the following standards and criteria:

1. These design criteria are applicable to the C-1 through C-3 zoning districts, excluding where located in the GGPOD.

#

5.05.01 - Businesses Serving Alcoholic Beverages

* * * * * * * * * * * * * *

- 7. The following uses shall be exempt from the requirements of section 5.05.01.
 - a. Any restaurant deriving at least fifty-one (51) percent of its gross revenue from the sale of food and nonalcoholic beverages.
 - b. Any motel and/or hotel with 100 or more guestrooms.
 - c. Any private club, golf club, country club, or civic or fraternal club may serve alcoholic beverages for consumption on-site when such service is incidental to the main use and for the exclusive use of the members, tenants, and/or guests of the facility.
 - d. Any beverage manufacturer having a tasting room with a floor area less than 10 percent of the gross floor area of the establishment.

#

5.06.02 - Development Standards for Signs within Residential Districts

* * * * * * * * * * * * * *

- 8. *Conditional uses within the residential and agricultural districts.*
 - a. Excluding properties located within the GGPOD, Properties granted conditional uses within the residential district are permitted one **wall sign** with a maximum of 32 square feet. Corner **lots** are permitted two such **wall signs**. Conditional uses allowed in the GGPOD pursuant to LDC section 2.03.07 F.7. shall follow the requirements for signs within nonresidential districts; however, the limitation on illumination as provided for in LDC section 5.06.02 B.7.a. shall be in effect.

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4 **10.03.06 - Public Notice and Required Hearings for Land Use Petitions**

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7

8 F. Variance, pursuant to LDC section 9.04.02, ~~or~~ a sign variance, pursuant to LDC
9 section 5.06.08, or a substantial deviation in the GGPOD, pursuant to LDC section
10 4.02.26:
11

- 12 1. The following advertised public hearings are required:
 - 13 a. One Planning Commission or Hearing Examiner hearing.
 - 14 b. If heard by the Planning Commission, one BZA hearing.
- 15 2. The following notice procedures are required:
 - 16 a. An Agent Letter shall be sent to property owners within 150 feet of the
 - 17 area covered by the petition following the initial staff review comments
 - 18 and prior to the resubmittal of the petition to the County.
 - 19 b. Mailed Notice prior to the advertised public hearing.
 - 20 c. Newspaper Advertisement prior to each advertised public hearing.
 - 21 d. Posting of a sign prior to the first advertised public hearing.

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Chapter 6. Waivers, Exemptions, and Reductions

#

J. Substantial Deviations in Golden Gate Parkway Overlay District

Reference LDC sections 2.03.07 F., 4.02.26 E., 5.06.04 B., and LDC Public Notice section 10.03.06 F.

Applicability Property owners in the Golden Gate Parkway Overlay District (GGPOD) may request a substantial deviation from specific dimensional or non-dimensional architectural, building, or site design standard of LDC section 4.02.26 and any standards listed as eligible for consideration in LDC section 4.02.26 E.1. Requested deviations that do not qualify as insubstantial deviations in accordance with LDC section 4.02.26 E.4. are considered substantial deviations.

All substantial deviation requests shall be made concurrently with an application for an SDP, SDPA, or SIP in the case of sign deviations, with a building permit. The applicant shall list all requested deviations on the required site plan(s) and depict the deviation(s) graphically on the plan(s). Additional graphic information may also be required by staff, on a case-by case basis.

Initiation The applicant files a "Substantial Deviations in Golden Gate Parkway Overlay District" application with the Zoning Division.

↔ See Chapter 1 D. for additional information regarding the procedural steps for initiating an application.

Pre-Application A pre-application meeting is required.

Application The application must include the following:

Contents

1. Applicant contact information.
2. Addressing checklist.
3. Electronic copy of all documents.
4. Property information, including:
 - a. Legal description;
 - b. Property identification number;
 - c. Section, township and range;
 - d. Subdivision, unit, lot and block, or metes and bounds description;
 - e. Acreage; and
 - f. Address of subject site and general location.
5. Zoning Information, including:
 - a. Zoning and land use of adjacent properties; and
 - b. Minimum yard requirements for subject property.
6. The name and mailing address of all registered Home Owners Associations and civic associations whose members are impacted by the application.

7. A detailed explanation of the request including:
 - a. Existing and proposed structures;
 - b. The amount of deviation to the standard that is proposed;
 - c. Survey of property illustrating the deviation (measured in feet);
 - d. Date of purchase by **property owner**;
 - e. The date the existing principal structure was built (include building permit numbers if possible);
 - f. Explanation of why substantial deviation is necessary;
8. Project narrative providing a detailed description/explanation of the substantial deviation, why it is requested, and the relevant criteria in **LDC** section 4.02.26 D.5.
9. An Official Interpretation or Zoning Verification Letter, if applicable.
10. Pre-application meeting notes.
11. A substantial deviation application shall include the Application Contents and Requirements for an SDP, SDPA, or SIP. ⇔ See Chapter 4.1.2 – 1.5 of the Administrative Code.
12. A substantial deviation application shall also include the following:
 - a. Submittal Credentials: Pursuant to **LDC** section 10.02.03, the engineering plans shall be signed and sealed by the applicant’s professional engineer licensed to practice in the State of Florida. For projects subject to **LDC** section 5.05.08, architectural drawings, shall be signed and sealed by a licensed architect, registered in the State of Florida. Landscape plans shall be signed and sealed by licensed landscape architect, registered in State of Florida.
 - b. Sheet size: The site improvement plan and the coversheet shall be prepared on a maximum size sheet measuring 24 inches by 36 inches, drawn to scale showing the areas affected by the amendment. The sheet must clearly show the change “clouded” and clearly delineate the area and scope of the work to be done.
 - c. A narrative of the redevelopment project and how it is consistent with the standards for approval, **LDC** section 4.02.26 E.
 - d. Description of each requested deviation and justification for each request. Requested deviations shall be clearly delineated in the petition. The **LDC** section for which the deviation seeks relief from shall be identified.
 - e. Project enhancements to offset or minimize the deviations shall be clearly identified.
13. A copy of the last recorded deed, contract for sale or agreement for sale, or a notarized statement of ownership.
14. An 8 ½ in. x 11 in. graphic location map of the site.
15. Aerial photographs (taken within the previous 12 months at a minimum scale of 1 in. = 200 ft.), showing FLUCCS Codes, legend, and project boundary.
16. Environmental Data Requirements. ⇔ See **LDC** section 3.08.00 A.
17. Owner/agent affidavit as to the correctness of the application.

18. Agent Letter review. Following the initial staff review comments and prior to the second submittal, the following Agent Letter materials shall be submitted to the assigned Planner for review and approval:

a. A list of the names and addresses of **property owners** to receive the Agent Letter; and

b. Draft of the Agent Letter.

19. Property Ownership Disclosure form.

20. Affidavit of Authorization.

Completeness and Processing of Application ⇔ See Chapter 1 D. for information regarding the completeness and processing steps of the application.

Notice Notification requirements are as follows. ⇔ See Chapter 8 of the Administrative Code for additional notice information.

1. Agent Letter: An Agent Letter shall be sent to property owners within 150 feet of the subject property following the initial staff review comments and prior to the second submittal. See Application Contents for review and approval of letter materials.

2. **Mailed Notice:** Written notice shall be sent to **property owners** in the notification area at least 15 days before the advertised Hearing Examiner hearing. The **mailed notice** shall be sent by the **applicant** following approval by the Zoning Division.

3. **Newspaper Advertisements:** The legal advertisement shall be published at least 15 days before the advertised Hearing Examiner hearing in a newspaper of general circulation. The advertisement shall include at a minimum:

a. Date, time, and location of the hearing;

b. Description of the requested variance; and

c. 2 in. x 3 in. map of the project location.

4. **Sign:** Posted at least 15 days before the advertised Hearing Examiner hearing. ⇔ See Chapter 8 of the Administrative Code for sign template.

Public Hearing The Hearing Examiner shall hold at least 1 advertised public hearing. ⇔ See Chapter 9 of the Administrative Code for the Office of the Hearing Examiner procedures.

Decision maker The Hearing Examiner.

Review Process The Zoning Division will review the application and identify whether additional materials are needed. Staff will prepare a Staff Report, utilizing criteria established in **LDC** section 4.02.26 E.5., to present to the Office of the Hearing Examiner for a decision.

K. Variance

(No proposed changes to variance)

#

ORDINANCE NO. 2018- 56

AN ORDINANCE OF THE BOARD OF COUNTY COMMISSIONERS OF COLLIER COUNTY, FLORIDA, ESTABLISHING THE GOLDEN GATE CITY ECONOMIC DEVELOPMENT ZONE; PROVIDING FOR THE CALCULATION OF AN ANNUAL TAX INCREMENT AMOUNT WITHIN THIS DEVELOPMENT ZONE; CREATING AN ECONOMIC DEVELOPMENT TRUST FUND FOR THIS DEVELOPMENT ZONE FOR THE TRANSFER AND MAINTENANCE OF SUCH TAX INCREMENT AMOUNTS; AUTHORIZING THE PLEDGE AND APPROPRIATION OF ECONOMIC DEVELOPMENT TRUST FUNDS; SETTING FORTH THE ECONOMIC DEVELOPMENT PLAN FOR THIS DEVELOPMENT ZONE; CREATING AND SETTING FORTH THE DUTIES OF THE GOLDEN GATE CITY ECONOMIC DEVELOPMENT ADVISORY BOARD; PROVIDING DEFINITIONS; PROVIDING FOR CONFLICT AND SEVERABILITY; PROVIDING FOR INCLUSION IN THE CODE OF LAWS AND ORDINANCES; AND PROVIDING AN EFFECTIVE DATE.

WHEREAS, in addition to its broad home rule powers, Collier County is expressly authorized under Section 125.045, Florida Statutes, to “expend public funds for economic development activities, including, but not limited to, developing or improving local infrastructure, issuing bonds to finance or refinance the cost of capital projects for industrial or manufacturing plants, leasing or conveying real property, and making grants to private enterprises for the expansion of businesses existing in the community or the attraction of new businesses to the community;” and

WHEREAS, Collier County previously adopted Ordinance No. 2010-20 providing for the creation of Innovation Zones, specifically finding as follows:

“It is the policy of the Board to promote economic growth which results in high wage jobs and helps diversify the economy of Collier County. To further this policy, it is the intent of the Board to create a dedicated source of revenue to fund an economic development program and to advance economic development initiatives in zones of geographic concentration within the unincorporated areas of the County. These zones, to be called Innovation Zones, will be designated by the Board from time to time through the implementation of Economic Development Plans adopted by resolution for each Innovation Zone;” and

WHEREAS, there is a compelling need for redevelopment within the commercial areas of Golden Gate City, whose specific needs require an approach that is substantially similar to, but not identical with, Innovation Zones as contemplated in Ordinance No. 2010-20; and

WHEREAS, recognizing the need to spur economic development in Golden Gate City, Collier County has already taken over the water and sewer service for Golden Gate City in part to provide commercial property owners a solution to development constraints caused by limited service; and

WHEREAS, Collier County wishes to continue to provide enhanced opportunities for economic development and redevelopment within the existing commercial areas within Golden Gate City.

NOW, THEREFORE, BE IT ORDAINED BY THE BOARD OF COUNTY COMMISSIONERS OF COLLIER COUNTY, FLORIDA, that:

Section One: Definitions and Findings

This Ordinance is intended to be a companion ordinance to Ordinance No. 2010-20, known as the Collier County Innovation Zone Ordinance. Accordingly, the Board of County Commissioners hereby adopts in full the definitions and findings set forth in Ordinance 2010-20, except for the Base Year Assessment Roll, which is defined below, and as modified hereby incorporates such definitions and findings as if specifically set forth below.

Section Two. Creation of the Golden Gate City Economic Development Zone.

The Board of County Commissioners hereby creates the Golden Gate City Economic Development Zone, which shall constitute the entirety of Golden Gate City, as graphically described in Exhibit A.

Section Three. Initial Tax Increment Year and Percentage.

For purposes of establishing the Base Year Assessment Roll, the base year shall be the last Collier County Real Property Assessment Roll certified by the Property Appraiser for the County Fiscal Year beginning October 1, 2015. Tax increments shall commence to be deposited into the Trust Fund with the County Fiscal Year commencing on October 1, 2019, and funding shall continue through September 30, 2029, unless extended by majority vote of the Board by Resolution. The amount of the tax increment to be deposited into the Trust Fund shall be equal to 100% of the amount based on the formula set forth in Section 4(B) of Collier County Ordinance No. 2010-20.

Section Four: Creation of an Economic Trust Fund.

The County hereby creates the Golden Gate City Economic Development Zone Trust Fund. The tax increment shall be deposited into the trust fund and the trust fund proceeds shall be utilized to implement the Economic Development Plan set forth in Section Five. The Trust Fund corpus will not exceed One Million Dollars (\$1,000,000) in any single fiscal year and is subject to annual appropriation by the County. Up to 5% of the Trust Corpus shall be reserved as an annual administrative fee for the County. Upon termination of the Trust Fund any unspent and unencumbered proceeds shall revert to the County's General Fund.

Section Five: Adoption of the Economic Development Plan.

The primary purpose of the Golden Gate City Economic Development Zone is to attract and retain qualified targeted industry business as defined by Florida Statute 288.106 and such businesses or industries identified by the Board of County Commissioners. Trust funds may be utilized in any lawful manner, including infrastructure required to serve new target businesses or the expansion of an existing target business; payment of County Impact Fees to be paid by the new target business or the expansion of an existing target business; and payment of building permit fees or other County fees related to the construction of structures to serve the target business. In addition to this primary purpose, trust funds may be utilized in any lawful manner which the Board of County Commissioners determines fosters economic development. Payment of funds are purely discretionary, and must be approved in advance by the Board of County Commissioners. All expenses must be fully documented in a manner acceptable to the County. Priority of funding will be for the redevelopment and renewal of the commercial district along Golden Gate Parkway, as graphically represented by Exhibit A.

Section Six: Creation of Golden Gate City Economic Development Zone Advisory Board.

The Golden Gate City Economic Development Zone Advisory Board (hereinafter referred to as the Advisory Board) is hereby established.

(a) Appointment and Composition. The Advisory Board shall be composed of seven (7) members who are appointed by resolution of the Board of County Commissioners, and shall be representative of the residential, business and commercial interests of the Golden Gate City Economic Development Zone. Non-residential members of the Advisory Board need not live within Golden Gate City, and upon recommendation of the Commissioner of the District, may reside outside of Collier County. Members of the Advisory Board shall be appointed by and serve at the pleasure of the Board of County Commissioners.

(b) Terms of Office. The initial terms of office of the Advisory Board members shall be one year for three members, and two years for four members, and each appointment or re-appointment thereafter shall be for two years. Appointments to fill any vacancies on the Advisory Board shall be for the remainder of the unexpired term of office.

(c) Removal from Office. Removal of members from the Advisory Board shall be in accordance with the provisions of Collier County Ordinance No. 2001-55, as it may be amended or by its successor ordinance.

(d) Officer; Quorum; Rules of Procedure. Annually the members of the Advisory Board shall elect a chairperson and vice chairperson from among the members. The chairperson's and vice chairperson's terms shall be for one year with eligibility for re-election by the members of the Advisory Board. The presence of fifty-one percent (51%) of the current voting membership, but never less than three total, shall constitute a quorum of the Advisory Board necessary to take action and transact business. In addition, an affirmative vote of a simple majority shall be necessary in order to take official action. Furthermore, by simple majority vote, but never with less than 5 members present, the Advisory Board shall adopt rules of procedure for its meetings, and thereafter shall be governed by its Procedures, as adopted and amended from time to time, subject to the approval of the Board of County Commissioners. The Advisory Board shall keep a written record of meetings, resolutions, findings and determinations. Copies of all Advisory Board minutes, resolutions, reports and exhibits shall be submitted to the Board of County Commissioners. The Advisory Board meetings shall be open to the public and conducted in the Sunshine.

(e) Reimbursement of Expenses. Members of the Advisory Board shall serve without compensation, but shall be entitled to receive reimbursement for expenses reasonably incurred in the performance of their duties upon prior approval of the Board of County Commissioners.

(f) Functions, Powers and Duties of the Advisory Board. The function, powers and duties of the Advisory Board shall be to aid and assist the Board of County Commissioners in carrying out the purpose of the Golden Gate City Economic Development Zone, including but not limited to advising and assisting the Board of County Commissioners in the establishment, review and enhancement of policies and programs to attract businesses or industries to the Golden Gate City Economic Development Zone, reviewing and making recommendations on applications for funding through the Trust Fund, as well as handling any matters that may be assigned by the Board of County Commissioners.

(g) Duties of the County Manager's Office. The County Manager's Office will provide such documentation, information, descriptions of procedures, secretarial support and general assistance to the Advisory Board as may be necessary for the Advisory Board to carry out its functions as set forth herein.

Section Seven: Conflict and Severability.

In the event this Ordinance conflicts with any other Ordinance of Collier County or other applicable law, the more restrictive shall apply. If any court of competent jurisdiction holds any phrase or portion of the Ordinance invalid or unconstitutional, such portion shall be deemed a separate, distinct and independent provision and such holding shall not affect the validity of the remaining portion.

Section Eight: Inclusion in the Code of Laws and Ordinances.

The provisions of this Ordinance shall become and may be made a part of the Code of Laws and Ordinances of Collier County, Florida. The section of the Ordinance may be renumbered or re-lettered to accomplish such, and the word "Ordinance" may be changed to "Section," "Article," or any other appropriate word.

Section Nine: Effective Date.

This Ordinance shall take effect upon filing with the Florida Department of State.

PASSED AND DULY ADOPTED by the Board of County Commissioners of Collier County, Florida, this 13th day of November, 2018.

ATTEST:
CRYSTAL K. KINZEL, Clerk

BOARD OF COUNTY COMMISSIONERS
COLLIER COUNTY, FLORIDA

By: [Signature]
Deputy Clerk

By: [Signature]
Andy Solis, Chairman

Attest as to Chairman's
signature only.

Approved as to form and legality:

[Signature]
Jeffrey A. Klatzkow, County Attorney

This ordinance filed with the
Secretary of State's Office the
14th day of November, 2018
and acknowledgement of that
filing, received this 14th day
of November 2018
By: [Signature]
Deputy Clerk



FLORIDA DEPARTMENT *of* STATE

RICK SCOTT
Governor

KEN DETZNER
Secretary of State

November 14, 2018

Ms. Crystal K. Kinzel, Clerk
Collier County
Post Office Box 413044
Naples, Florida 34101-3044

Attention: Ann Jennejohn

Dear Ms. Kinzel:

Pursuant to the provisions of Section 125.66, Florida Statutes, this will acknowledge receipt of your electronic copy of Collier County Ordinance No. 2018-56, which was filed in this office on November 14, 2018.

Sincerely,

Ernest L. Reddick
Program Administrator

ELR/lb

REVISED 03/2011

Community
Roadways
Beautification
Master Plan



Golden Gate Community Roadways **Beautification Master Plan**

Prepared For:

Collier County
Board of County Commissioners

and

Golden Gate Beautification
Municipal Services Taxing Unit, M.S.T.U.
Advisory Committee

Prepared By:

McGee & Associates

Landscape Architecture

5079 Tamiami Trail East
Naples, Florida 34113
(239) 417-0707

COLLIER COUNTY BOARD OF COUNTY COMMISSIONERS

Honorable Fred W. Coyle, District 4, Chairman

Honorable Jim Coletta, District 5, Vice Chairman

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INTRODUCTION

Collier County Government, along with the residents of the Golden Gate Community, through a public/private partnership since 1983, have been working together to perform roadway landscape beautification within the Golden Gate Beautification Municipal Services Taxing Unit (M.S.T.U.). To date this partnership has accomplished many of its initial goals and completed six (6) miles of roadway median landscape beautification within its M.S.T.U. boundary. The completed roadways are Golden Gate Parkway east of Santa Barbara Blvd., Collier Blvd., Tropicana Blvd. and Sunshine Blvd.

The residents of the Golden Gate Beautification M.S.T.U. recognized the need and economic benefits associated with roadway landscape beautification. The Golden Gate Beautification M.S.T.U. is a local precinct voter referendum ad valorem taxing district established for roadway corridor beautification and maintenance purposes. The M.S.T.U. was established through Collier County Ordinances (No.83-55, Rev.96-51 & No.87-78, Rev.96-50), and is under the authority of the Board of County Commissioners. The Board of County Commissioners appoints a five member Advisory Committee consisting of residents that reside within the M.S.T.U. beautification district boundaries. The Committee makes recommendations to the Board of County Commissioners on the operation and budgeting of the M.S.T.U. on a monthly basis. The day-to-day administration of the M.S.T.U. is administered through the Collier County Transportation Services Division, Alternative Transportation Modes Department.

The Advisory Committee, after completing the initial Golden Gate Parkway median landscape beautification project, concluded that a Master Plan encompassing all the community roadways was needed in order to properly understand and guide the Committee and Community in proceeding with present and future beautification projects.

This Roadway Beautification Master Plan will provide the guidance, by which the Golden Gate Community will significantly enhance its image while creating a vision for the future. Aesthetically enhanced and well maintained roadways are the first image of a Community's economic vitality and self pride. Landscaped roadway corridors help to increase property values and encourage new residents and businesses to locate within a Community.

OVERVIEW

For purposes of this Master Plan the Golden Gate Community will consist of a 4 square mile unincorporated area of Collier County. The Community contains a community center, shopping centers, recreation facilities, schools, library, fire station, police facilities, and government meetings center. The Beautification M.S.T.U. District also contains local businesses, single and multi-family residents within the Community.

The Golden Gate Beautification M.S.T.U. District encompasses a 4 square mile area known as Golden Gate City (See Figure 1). The M.S.T.U. District is located approximately 4.5 miles east of the City of Naples. The M.S.T.U.'s eastern boundary is the eastern right-of-way of (C.R. 951) Collier Blvd.; the northern boundary is the north right-of-way of Green Blvd.; the west boundary is the west right-of-way of Santa Barbara Blvd.; and the south boundary is the Golden Gate Canal. The M.S.T.U. District is bordered to the north, east and west by an area known as Golden Gate Estates which consist dominantly of residentially zoned property. To the south by the Golden Gate Canal which is abutted by the Golden Gate Regional Park, Golden Gate High School and several residential planned unit developments.

A major influence upon the Golden Gate Community is the Interstate 75 roadway corridor which passes within less than one mile of the M.S.T.U.'s south and west boundary lines. There are three Interstate 75 interchanges (Pine Ridge Road, Golden Gate Parkway and Collier Blvd.) that greatly impact this community.

Bisecting the M.S.T.U. District from east to west is the Golden Gate Parkway roadway corridor which is a major east/west traffic corridor from the City of Naples and the Interstate 75 and Golden Gate Parkway interchange into the Golden Gate M.S.T.U. Community. This roadway has been designated with a Parkway status and listed as a major corridor roadway in the "Corridor Management Study" dated 1988 and the "Collier County Streetscape Master Plan" dated 1997. In 2006 the eastern portion of Golden Gate Parkway within the M.S.T.U. District received special development designations through the adoption of the Golden Gate Master Plan.

In May of 1996 the Beautification Advisory Committee voted to develop and began a process through public meetings/workshops, working with County staff and the District landscape architectural consultant to create a Community Roadways Beautification Master Plan. The plan would address the County and Community major internal and external arterial and collector

roadways that are within or would have a major influence on the M.S.T.U. District.

The Master plan addresses the following elements:

1. Goals and Objectives
2. Site Inventory and Analysis
3. Concept Development
4. Written and graphic guidelines to assist in the Community's roadway beautification efforts.
5. Cost Budgeting and Implementation Phasing

The Master plan is based on the concerns, observations and desires identified by the general Public, Residents, Civic organizations, Advisory Committee, County staff and Consultants. In order to provide for roadway beautification and continued landscape maintenance to enhance the Community's image, pride, and economic vitality while providing for pedestrian and vehicular safety.

HISTORICAL REFERENCES

On May 27, 1997, the Board of County Commissioners (BCC) considered Agenda Item 8 (B), (1) which recommended the approval of the Golden Gate Community Roadways Beautification Master Plan, accept the annual maintenance cost for the Golden Gate Parkway and to enter into a funding partnership for the beautification of the medians on CR 951 (Collier Blvd.) and north Santa Barbara Blvd. within the boundaries of the MSTU. The BCC approved the Master Plan concept, agreed to take over the annual maintenance costs on the Golden Gate Parkway and directed that the beautification of medians CR 951 and Santa Barbara Blvd. be considered for future budget cycles. In page eight of the executive summary the cost estimates for Santa Barbara Phase 1 and Phase 2 provided to the BCC for north Santa Barbara Blvd. from Golden Gate Canal to Green Blvd.

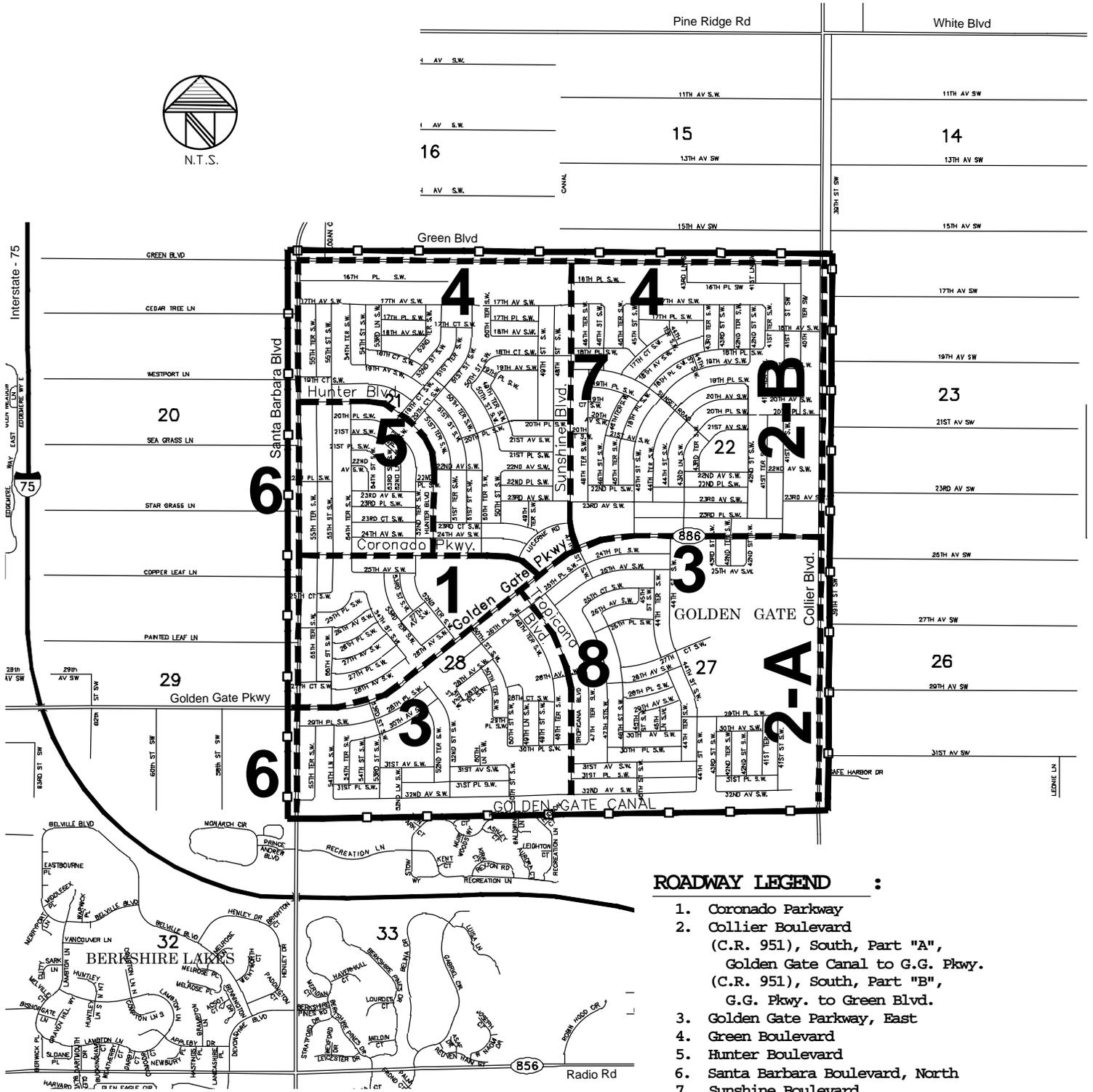
On February 3, 1998 Agenda Item 16(B), (5) The BCC approved " Consider a County Arterial Roadway Landscape Improvement Funding Partnership with the Golden Gate Beautification MSTU for CR 951 where the Golden Gate MSTU requested a two year funding partnership for CR 951. The Golden Gate MSTU funded the median improvements on the southern portion of CR 951 (Part "A") at the estimated cost of \$213,000 during the Fiscal Year 1997- 1998 and that the County would fund the median landscape improvements on the northern portion of CR 951 (Part "B) at the cost of \$294,000. The County would then assume the annual maintenance of costs for both sections estimated at \$132,000 per year.

The Board of County Commissioners in 2003 incorporated the "Golden Gate Community Roadways Beautification Master Plan" into the Collier County Land Development Code.

In April 2003 the Board of County Commissioners approved the "Collier County Beautification Master Plan" which addressed the existing and future roadway landscape beautification and maintenance of the arterial roadways within the Golden Gate Community 4 square mile area addressed by this Master Plan. The roadways affected by the "Collier County Beautification Master Plan" are Golden Gate Blvd., Collier Blvd., Pine Ridge Road, Logan Blvd., Santa Barbara Blvd., Golden Gate Parkway west of Santa Barbara Blvd. and the Interstate 75 interchanges.

Based upon the above the Golden Gate Beautification M.S.T.U. Advisory Committee, in 2006 recommended an updating of this Master Plan.

MASTER PLAN ROADWAYS MAP



ROADWAY LEGEND :

1. Coronado Parkway
2. Collier Boulevard
(C.R. 951), South, Part "A",
Golden Gate Canal to G.G. Pkwy.
(C.R. 951), South, Part "B",
G.G. Pkwy. to Green Blvd.
3. Golden Gate Parkway, East
4. Green Boulevard
5. Hunter Boulevard
6. Santa Barbara Boulevard, North
7. Sunshine Boulevard
8. Tropicana Boulevard

LEGEND:

- BEAUTIFICATION MASTER PLAN ROADWAYS
- BEAUTIFICATION M.S.T.U. BOUNDARY

GOALS and OBJECTIVES

The Master plan goals and objectives for roadway landscape beautification strived to established the purposes for roadway beautification while encouraging the community and government to work together in order to reach the goals. The goals and objectives were developed to encourage the community to strive beyond the norm while being realistic in scheduling and cost. Goals and objectives should not be considered permanent, but must be reviewed and reset as they are reached or change. The objectives will establish the directions or means by which the community will follow in achieving the goals. The objectives should be considered directives and guidelines which are transformable into policies or programs.

GOAL #1 Enhance the visual quality and economic vitality of the community through roadway corridor beautification while improving the health, safety, welfare, and quality of life for the residents and visitors.

Objectives 1.1:

Encourage adjacent roadway property owners to install, upgrade and maintain roadside landscaping by providing assistance and landscaping expertise to the property owner.

Objectives 1.2:

Request and assist Collier County Government in implementation and enforcement of the Land Development Code requirements for installation and maintenance of landscape buffers adjacent to roadway corridors. Petition the Board of County Commissioners to allow the Beautification Advisory Committee to review and provide recommendation on proposed development landscape plans in order to assure coordination with the Community's Roadway Landscape Master Plan.

Objectives 1.3:

Encourage Collier County Government, property Owners, businesses and community developments within and beyond the M.S.T.U. boundaries to participate in landscape beautification of the roadway corridors located adjacent to their property.

Objectives 1.4:

Enhance Community identity and unity by developing a set of coherent beautification guidelines and standards for roadway corridors. This can be accomplished through the selection and coordination of lighting fixtures, site furnishing, plant materials and roadway signage.

GOAL #2 **Develop a Roadway Landscape Beautification Master Plan which will guide the Community, Collier County Government and the Beautification Advisory Committee in implementing future roadway beautification projects within and adjacent to the Golden Gate Community.**

Objectives 2.1:

Incorporate South Florida Water Management District's, South Florida Edition "WATERWISE Florida Landscapes" guide to promote water conservation using the Principles of Xeriscape™.

Objective 2.2:

Incorporate and/or expand upon existing County codes, standards or guidelines, as well as the Golden Gate Master Plan polices relating to landscaping of adjacent roadway properties and the roadway corridors.

Objective 2.3:

Utilize native or naturalized Southern Florida plant species.

Objective 2.4:

Research and analyze the availability and use of reused water for roadway landscape irrigation.

Objective 2.5:

Improve pedestrian/bicycle circulation and facilities.

Objective 2.6:

Improve and expand street lighting for vehicular and pedestrian safety.

Objective 2.7:

Review traffic calming techniques and devices for possible implementation on proposed and existing roadways.

Objective 2.8:

Incorporate the Florida Department of Transportation landscape standards and guidelines to the greatest extent practical into the proposed Master Plan standards.

Objective 2.9:

Increase and enhance visibility of pedestrian roadway crossings for safety purposes.

Objectives 2.10:

Develop beautification improvements that provide for reduced long range maintenance.

Objectives 2.11:

Develop a Master Plan that will allow for roadway beautification capital improvements and maintenance of roadways within the M.S.T.U. based upon the existing tax millage revenues.

GOAL #3 **Develop a phasing and funding strategy for the implementation of the Community roadways landscape beautification programs and projects.**

Objectives 3.1:

Encourage continuation of the public/private partnership with Collier County Government. Assist Collier County Government in expediting its landscape development plans for arterial roadway corridors which run through the Beautification M.S.T.U. and community.

Objectives 3.2:

Encourage community organizations and residents to participate beyond the existing beautification tax revenues by providing additional funding and support to the beautification efforts.

Objectives 3.3:

Analyze the consequences of expanding the M.S.T.U. boundaries for additional areas beautification and funding.

Objectives 3.4:

Identify and review Government grants, programs or funds that could provide assistance or funding for beautification.

Objectives 3.5:

To convince Collier County Board of County Commissioners to accept and fund landscape maintenance responsibility on the major county arterial roadways within the M.S.T.U.

Objectives 3.6:

Assist the Collier County Government in identifying arterial and collector roadways that are in need of roadway maintenance to address resurfacing, restriping, and drainage improvements for the safety of the residents and users.

EXISTING CONDITIONS and CONSTRAINTS

Project Area

The M.S.T.U. district includes 12(+/-) miles of Collier County arterial and collector roadway corridors. All the roads are four lane median divided roadways, except for the western one mile of Green Blvd. which is a two lane road.

Climate

The Golden Gate area would be described as a typical inland subtropical climatic zone. The area is subject to yearly frost and freezing temperatures of 26 to 28 degrees. The coldest temperatures generally occur during the winter months of December, January and February. During the summer months of June through September the area is exposed to high temperatures and humidity levels in the 80 and 90 percent range. The annual rainfall exceeds 50 inches with 75 percent occurring during the summer months of June through September. The Golden Gate area during the peak of the rainy season experiences severe thunderstorms that generate damaging winds and heavy rain fall up to 3 to 4 inches per storm. The roadway medians have micro-climates created within them due to the large amounts of pavement surrounding them. Road temperatures can reach well above 100 degrees during the day and maintain over a 100 degrees well after sunset. These above conditions combined with the auto exhaust, water pollution, blown debris and increased wind generated from the passing vehicles creates a very harsh environment within the medians for plants to survive in.

Soil and Topography

The roadway corridors are generally level with elevation changes of four feet or less. The roadway is typically crowned to the center to create a positive drainage pattern to the right-of-way swales or curbed storm water drainage system. As typical with most roadways, the soil used to raise the elevation and construct the roadway were intermixed and imported from other areas of the county. The soil within the roadway medians is typically an intermixed stabilization material containing large amounts of crushed limestone material and sand. It has a high PH level of 8 or above. These soils are susceptible to compaction, hydrophobic, poorly aerated and poor percolation. They have limited water and nutrient holding capacities making them unsuitable for landscape purposes. Sporadically throughout the Golden Gate M.S.T.U area is a sub-surface layer of cap rock or

limestone. It ranges in depth from 18 to 36 inches below the surface.

It is recommended prior to excavation or installation of any utilities below 18 inches, that test borings be performed. In most cases soil amendments or excavation to add new soil will be required in order to provide improved soil conditions for landscaping.

Area Vegetation

The roadway rights-of-way corridors have been totally cleared of any existing native vegetation. The dominate vegetation community surrounding the area is a pine flat-wood forest and transitional pine/cypress forest. The dominant tree species is the slash pine and the dominant under-story vegetation is the saw palmetto with intermixed pond cypress, Dahoon holly and Wax myrtle. Much of the area vegetation is in transition from wetlands to uplands due to the past alteration of the surface water and natural drainage.

Water Resources for Irrigation

There are four main water supply sources within the Golden Gate area that can be used for landscape irrigation purposes.

1. Well
2. Surface Water (canal)
3. Reclaimed Wastewater
4. Potable or Treated Water

The following is a list of advantages and disadvantages for each source:

Well System

Advantage:

Location flexible requiring minimum piping, no continuous water cost, reduced overall system cost.

Disadvantage:

Permitting required, water could contain iron & sulfur, limits on gallons pumped per day, no backup when problems occur, annual metering cost, subject to water restrictions, reduces a limited natural resource.

Surface Water, (Canal)

Advantage:

Source located close to all project site areas, low start up cost, no continuous water cost, reduced overall system cost.

Disadvantage:

Additional permitting cost required; no continuous recharge source; demand would lower canal water levels; uncontrolled potential contamination; annual metering cost. The source and level of water in the canals is dependent on rainfall which occurs in this area during the months that irrigation requirements is typically less, subject to water restrictions, reduces limited natural resource.

Reclaimed Wastewater

Advantage:

Reuse of natural resource, low cost per gallon, provides outlet for wastewater treatment facilities to discharge reclaimed water. Florida Cities Wastewater treatment facility is located within M.S.T.U. boundary and could supply quantity required. A Collier County reclaimed water pipe line existing at the intersection of Collier Blvd. and Vanderbilt Beach Rd. Reuse water can also be applied by truck and use of reclaimed water is an environmentally friendly irrigation method.

Disadvantage:

High initial start up cost for a piping system within the M.S.T.U. area; Florida Cities' plant must be upgraded in order to supply proper quality of water; a 3 mile pipe line must be installed in order to connect to Collier County's reclaimed water discharge system; reuse water quantity available at time of year when watering is in demand.

Potable or Treated Water

Advantage:

Initial low start up cost; clean water source; source close to project area requiring minimum piping.

Disadvantage:

High per gallon cost, not available to all project areas, subject to water restrictions, reduces limited natural resource.

The following written roadway descriptions and pictorial cross sections are of all the major County arterial and collector roadways within the 4 square mile Golden Gate Community area.

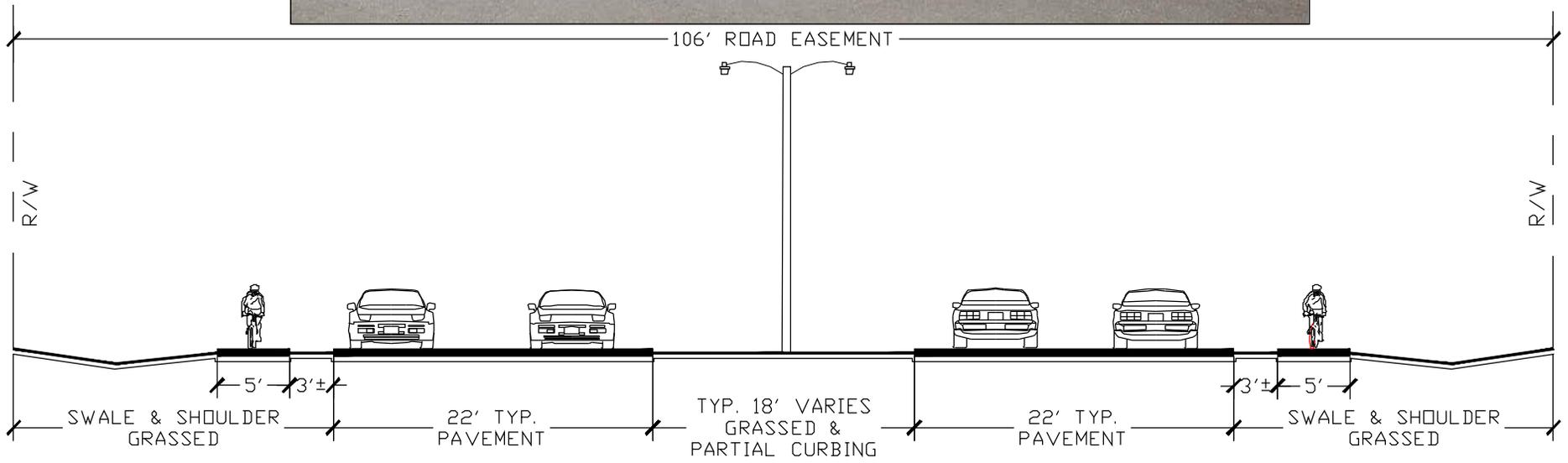
The written descriptions are an assessment of the existing conditions and contain a brief description about the roadway, signalized intersections, curbing, median description, side right-of-way areas, roadway signage, overhead utilities, roadway lighting, pedestrian walkways, bike pathways, and roadside development.

The pictorial cross sections locations were selected to show the best typical example of the existing roadway and dimensions of the existing rights-of-way and roadway facilities.

<u>Roadway Number</u>	<u>Name</u>
1	Coronado Parkway
2	C.R. 951, South (Golden Gate Canal to Green Blvd.)
3	Golden Gate Parkway, East (C.R. 951 to Santa Barbara Blvd., North)
4	Green Blvd.
5	Hunter Blvd.
6	Santa Barbara Blvd., North (Golden Gate Canal to Green Blvd.)
7	Sunshine Blvd.
8	Tropicana Blvd.

Coronado Parkway Existing Conditions
(Golden Gate Parkway, East to Santa Barbara Blvd., North)

- Roadway descriptions:** A .90 mile roadway running east and west within a 106 foot wide road right-of-way easement. There are two 2 lane bridges over the Coronado Canal at the western end of the roadway between 54th Terr. SW and 55th St. SW. The road is an uncurbed divided four lane roadway. The posted speed limit is 30 MPH. This road is a County collector roadway.
- Signalized intersection:** Golden Gate Parkway
- Curbing:** The medians are mostly uncurbed, except for turn lanes and the median noses at the shopping center entrances along with the first median at the intersection with Golden Gate Parkway. The curb is mountable type "A" concrete curb.
- Median Description:** There exist .74 miles of median green area divided into 13 medians. The medians are grassed and generally level with the pavement. The medians range in width from 8 to 9 feet at turn lane locations to 18 feet.
- Side Rights-of-Way:** The north and south sides have grassed utility strips, 5 foot wide asphalt and/or concrete sidewalks and then a grassed swale adjacent to the right-of-way lines.
- Roadway signage:** The roadway signage consists of metal traffic and road identification signs mounted on 4" x 4" wood post.
- Overhead utilities:** There are overhead electric transmission lines running along the roadway behind the shopping center. Underground electric is present within medians supplying the street lights.
- Roadway lighting:** There are no street lights continuously along the roadway, only at the intersections with other roadways. The street lights are located in the medians and are double standard street light fixture mounted on top of a concrete pole.
- Pedestrian walkways:** There are 5 foot wide asphalt and/or concrete sidewalk along the north and south rights-of-way.
- Bike pathways:** Designated off-road, 5 foot wide facility shared with pedestrians.
- Roadside Development:** Along the south right-of-way line starting at the intersection with Golden Gate Parkway the properties are commercially developed to 52nd Terr. SW, from there to one half-block back from Santa Barbara Blvd. is single family and duplex residences. The half block adjacent to Santa Barbara Blvd. is developed as commercial. Along the north right-of-way line starting at the intersection with Golden Gate Parkway it is developed with Public facilities to Lucerne Road, from Lucerne road to one half-block back from Santa Barbara Blvd. is single family, multi-family and duplex residences. The remaining half-block is developed as commercial.



TYPICAL EXISTING CORONADO PARKWAY

Collier Blvd. (C.R. 951.) South Existing Conditions
(Golden Gate Canal to Green Blvd.)

- Roadway descriptions:** A 2 mile roadway running north and south within a 100 foot wide road right-of-way easement. This roadway will be subdivided into two smaller sections. Part "A" is the southern one mile south of Golden Gate Parkway and Part "B" is the northern one mile north of Golden Gate Parkway. Both part "A" & "B" are divided four lanes with curbed medians and curbed outside traffic lanes. The posted speed limit is 45 MPH. The entire roadway is a major County, Community and Golden Gate City arterial roadway. The southern end of Part "A" ends at the first Naples northbound Interstate 75 interchange. This roadway is a major entrance into the Golden Gate Community. Part "B" roadway is built 50 feet (+/-) into the existing 100 foot canal easement.
- Signalized intersection:** Golden Gate Parkway
- Curbing:** Part "A": Medians and outside traffic lanes are all curbed with vertical type "F" curb and gutter.
Part "B": Medians and outside lanes are all curbed. The 3 medians between Golden Gate Parkway and 20th Pl. have vertical type "F" curb. The remaining medians from 20th Pl. to Green Blvd. have mountable type "E" curb. The outside traffic lanes are curbed with vertical type "F" curb and gutter. The medians with turn lanes built in them have solid concrete separators the distance of the stacking area of the turn lane.
- Median Description:** Part "A": There exist .89 miles of median green area. The medians are Improved with landscaping and irrigation. The medians Generally are 18 feet wide.
Part "B": There exist .90 miles of median green area. The medians are improved with landscaping and irrigation.
- Side Rights-of-Way:** Part "A": The west side right-of-way is a grassed shoulder that slopes into a drainage swale. On the east side of the right-of-way directly behind the curb is a standard metal guardrail that extends the full length of the section of roadway. From the back of curb, under the guardrail, a distance of four feet (+/-) is asphalt pavement. From the end of the pavement under the guardrail starts a grassed canal bank.
Part "B": The west side right-of-way is grassed with a 5 foot sidewalk along the right-of-way line. The grassed area is either level with back of curb and/or sloping into a drainage swale. The east side right-of-way has a grassed retention swale varying in width starting at the back of curb. This swale is prior to the canal top of bank. This retention swale handles roadway drainage prior to flowing into the adjacent canal.
- Roadway signage:** The roadway signage consists of metal traffic and road identification signs mounted on 4" x 4" wood post and metal poles.

- Overhead utilities:** Both Parts A & B have overhead electric transmission lines along the west right-of-way line.
- Roadway lighting:** Part "A": There are no street lights continuously along the roadway only at the intersections with other roads.
Part "B": There are street lights continuously down both sides of the roadway. The street lights are standard mansard arm metal poles with cobra head light fixtures.
- Pedestrian walkways:** Part "A": There are no sidewalks on either side of the roadway.
Part "B": There is sidewalk along 95% of west right-of-way.
- Bike pathways:** Part "A": There are no off-road or on-road designated facilities.
Part "B": There is a 5 foot off-road facility shared with pedestrians.
- Roadside Development:** Part "A": Along the east right-of-way line is a 100 foot drainage canal easement. Along the west right-of-way line from the Golden Gate canal to a point 500 feet (+/-), before the intersection with Golden Gate Parkway is a golf course facility. The remaining 500 feet is undeveloped commercially zoned property.
Part "B": Along the east right-of-way line is a 100 foot drainage canal easement. The west right-of-way has heavy roadside commercial development existing and the remaining vacant property is zoned for continued commercial development.
- Landscape Development:** **Parts "A" and "B" median landscape and irrigation is complete.**



IMPROVED MEDIANS - COLLIER BLVD. (C.R.951), PART "A"



IMPROVED MEDIANS - COLLIER BLVD. (C.R. 951) PART "B"

Golden Gate Parkway, East Existing Conditions
(C.R. 951, South to Santa Barbara Blvd., North)

Roadway descriptions:	A 2.5 mile roadway running east and west within a 130 foot wide road right-of-way easement. The road is a curbed divided four lane roadway. The posted speed limit is 35 MPH. This roadway is a major County, Community and Golden Gate east / west arterial roadway. This roadway is the main roadway through the Golden Gate Community from the Naples Urban area.
Signalized intersection:	Collier Blvd. (C.R. 951), 44 th St., Sunshine Blvd., Coronado Pkwy., Tropicana Blvd., 50th St. S.W., 53 rd St., Santa Barbara Blvd.
Curbing:	The medians are curbed with mountable type "A" curbing.
Median Description:	There exist 1.9 miles of median green area. The medians are completely landscaped and irrigated. The medians range in width from 10 feet to 38 feet (+/-) at the widest points.
Side Rights-of-Way:	The north and south sides have grassed utility strips, 5 foot wide asphalt and/or concrete sidewalks and then a grassed swale adjacent to the right-of-way lines.
Roadway signage:	The roadway signage consists of metal traffic and road identification signs mounted on 4" x 4" wood post.
Overhead utilities:	There is overhead electric transmission lines running along the right-of-way lines. There exist electric underground and over head lines within the medians that service the street lights located in the medians.
Roadway lighting:	The western portion of the roadway has street lights along the right-of-way lines. The eastern portion has double headed street lights located within the medians. The lights are standard cobra head fixtures mounted on concrete poles.
Pedestrian walkways:	There are 5 foot wide asphalt and/or concrete sidewalks along the north and south rights-of-way.
Bike pathways:	Designated off-road, 5 foot wide facility shared with pedestrians.
Roadside Development:	The properties on the eastern half of the roadway from Sunshine Blvd. to Collier Blvd. is about 98% developed with multi-family and duplex residential along both sides of the roadway. Commercial development occurs within the last block adjacent to C.R. 951. The western half of the roadway is about 90% developed with highway commercial and shopping center complexes. The remaining properties are undeveloped, but zoned for commercial uses.
Landscape Development:	The median landscape and irrigation is complete.



IMPROVED MEDIANS - GOLDEN GATE PARKWAY EAST

Green Blvd. Existing Conditions

(Collier Blvd. (C.R. 951) to Logan Blvd. & Santa Barbara Blvd. North Intersection)

- Roadway descriptions:** A 2 mile roadway running east and west within a 100 foot wide road right-of-way easement. The eastern portion from Collier Blvd. to Sunshine Blvd. is an uncurbed divided four lane roadway. The western portion from Sunshine Blvd. or the bridge to Logan Blvd. is an uncurbed two lane roadway. The western two lane portion is offset to the north side of the right-of-way easement. The posted speed limit is 40 MPH. This road is a County arterial / collector roadway along the north boundary of the Golden Gate M.S.T.U.
- Signalized intersection:** Santa Barbara Blvd. North, Sunshine & Collier Blvd.
- Curbing:** The first median at the intersection with Collier Blvd. is curbed with Type "D" concrete curb. The remaining medians are only curbed on the noses with type "D" curb at the median openings.
- Median Description:** There exist .90 miles of median green area divided into 5 medians. within the eastern half of the roadway. The medians are grassed and generally level with the pavement. The medians have a typical width of 19'-6" (+/-). The western half of the roadway has no medians.
- Side Rights-of-Way:** The sides are typical grassed shoulders sloped down into a swale system adjacent to the right-of-way lines.
- Roadway signage:** The roadway signage consist of metal traffic and road identification signs mounted on 4" x 4" wood post.
- Overhead utilities:** Overhead electric transmission lines run down the northern right-of-way line about 95% of the roadway length.
- Roadway lighting:** There are no street lights continuously along the roadway only at the intersections with other roadways.
- Pedestrian walkways:** From Collier Blvd. to Sunshine Blvd. there is a in road striped pedestrian area along the south and north side of the road. From Sunshine Blvd. to Santa Barbara Blvd. there is a 5 to 6 foot wide asphalt and concrete sidewalk along the south right-of-way line.
- Bike pathways:** Designated in-street, 7-8 foot wide asphalt facility on both sides of the four lane eastern portions of the roadway. Designated off-street, 5 to 6 foot wide facility shared with pedestrians along the eastern two lane portion.
- Roadside Development:** The property along the north side of the roadway is zoned Estates District. The properties along the south side of the roadway are over 50% developed with multi-family residential units. The properties on the north and south sides of the roadway are zoned commercial between Collier Blvd. to 40th Ter. SW, but only the south side is developed.



EXISTING UNIMPROVED MEDIANS - GREEN BLVD. EAST PORTION



EXISTING UNIMPROVED MEDIANS - GREEN BLVD. WEST PORTION

Hunter Blvd. Existing Conditions
(Coronado Parkway to Santa Barbara Blvd., North)

Roadway descriptions:	A .90 mile roadway running northeast and southwest within a 106 foot wide road right-of-way easement. The road is a mostly uncurbed divided four lane roadway. The posted speed limit is 25 MPH. This road is a minor County collector roadway.
Signalized intersection:	West end at Santa Barbara Blvd., North
Curbing:	The medians are uncurbed.
Median Description:	There exist .84 miles of median green area divided into 11 medians. The medians are grassed and generally level with the pavement or curb. The medians range in width from 8-9 feet at turn lane locations to 19'-6" (+/-) at the widest locations.
Side Rights-of-Way:	The side rights-of-ways are grassed shoulders sloped into a drainage swale.
Roadway signage:	The roadway signage consists of metal traffic and road identification signs mounted on 4" x 4" wood post.
Overhead utilities:	There are no overhead electric transmission lines along the roadway. Underground electric is present within some medians supplying the street lights.
Roadway lighting:	There are no street lights continuously along the roadway, only at the intersections with other roadways. The street light fixtures are located in the medians and are double standard street lights mounted on top of a concrete pole.
Pedestrian walkways:	There exist a 5 foot wide sidewalk along the east and north sides of the roadway.
Bike pathways:	Two lane roadway in-road striped bike paths along both sides of the roadway.
Roadside Development:	Along the northeastern right-of-way line starting at the intersection with Coronado Parkway is 90% developed with single family or duplex residences. Between 55th Ave. SW and Santa Barbara Blvd. North the properties are developed with office and commercial facilities. The southwestern side of the roadway is developed to the extent and with same types of developments as the northeastern side.



EXISTING UNIMPROVED MEDIANS - HUNTER BLVD.

**Santa Barbara Blvd., North Existing Conditions
(Golden Gate Canal to Green Blvd.)**

- Roadway descriptions:** A 2.1 mile roadway running north and south along the west boundary of the Golden Gate Beautification M.S.T.U. There are 2 bridges over the Green and Golden Gate Canals. The northern portion north of Coronado Blvd. is a mostly uncurbed divided four lane roadway. The southern portion (4,270 ln. ft. +/- north of the Golden Gate Canal) is an improved 6 lane major arterial roadway with Type F curbing. The posted speed limit is 45 MPH. This roadway is a major County, Community and Golden Gate M.S.T.U. arterial roadway.
- Signalized intersection:** Golden Gate Parkway & Green Blvd. (Golden Gate Parkway intersection is designated a major intersection per the 1988 "Corridor Management Study")
- Curbing:** The northern portion medians are generally uncurbed, except for the last 4 medians north and south of the Green canal bridge with mountable type "A" curb. The outside lanes between 18th Ave. SW and Green Blvd. are curbed with vertical type "F" curb. The 8 medians and outside lanes of the southern portion have Type F curbing.
- Median Description:** There exist 1 mile of median green areas divided into 21 medians. The northern 13.5 medians are grassed, uncurbed and level with the pavement. The medians range in width from 7 feet to 18 feet. The southern 7.5 medians are curbed, filled and grassed. The southern portion medians contain 73,669 sq. ft. +/- of green median area.
- Side Rights-of-Way:** Both sides are grassed shoulders sloped into a drainage swale.
- Roadway signage:** The roadway signage consists of metal traffic and road identification signs mounted on standard metal and/or 4" x 4" wood post.
- Overhead utilities:** There are overhead electric transmission lines crossing and along the west right-of-way line.
- Roadway lighting:** The northern portion has no street lights continuously along the roadway. The southern portion has street lights.
- Pedestrian walkways:** The northern 6,764 ln. ft. +/- of the roadway has existing in-road and off road sidewalk facilities along the east side of the roadway. The southern 4,270 ln. ft. +/- of roadway has concrete sidewalks along both sides.
- Bike pathways:** The northern 6,764 ln. ft. +/- of the roadway has one in-road facility along the east side. The southern improved 6 lanes has in-road bike lanes.
- Roadside Development:** The property along the west side of the roadway is generally zoned Estates District and is about 25% developed with single family residences. South of Golden Gate Parkway the properties are 50% developed with private schools and church facilities. The remaining properties are undeveloped. The east side of the roadway is developed with a mix of commercial and multi-family properties.
- Landscape Development:** **The future landscape development of this section of roadway is listed to be implemented under the Collier County Landscape Beautification Master Plan. As of May 2009 the Collier County Beautification Master Plan has this roadway unfunded.**



Northern Section - Unimproved Four Lane (Approx. Coronado Pkwy. to Green Blvd.)

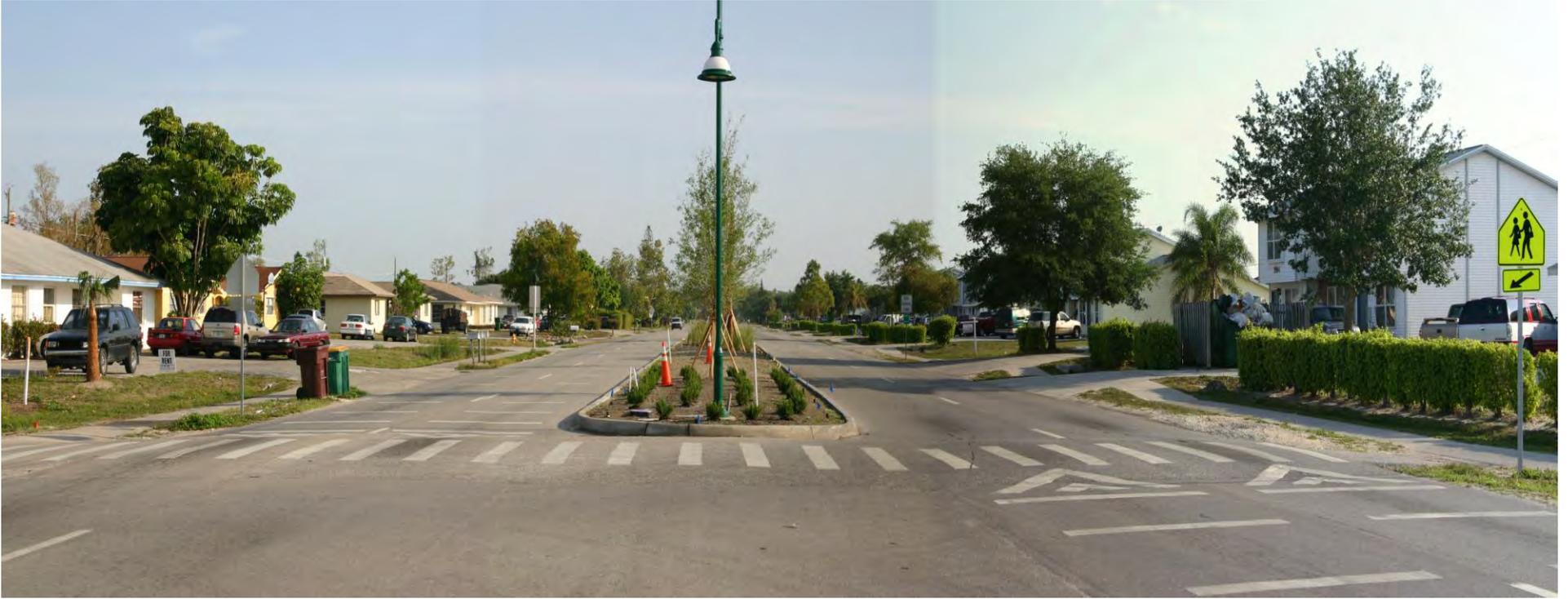


Southern Section - Improved Six Lane (Approx. Golden Gate Canal to Coronado Pkwy.)

UNIMPROVED AND IMPROVED MEDIANS - SANTA BARBARA BLVD. NORTH

Sunshine Blvd. Existing Conditions
(Golden Gate Parkway, East to Green Blvd.)

Roadway descriptions:	A 1.1 mile roadway running north and south within a 106 foot wide right-of-way easement. The northern most 850 feet of the roadway is a typical asphalt pavement two lane road off set to the east side of the right-of-way easement. There is a two lane bridge in this most northern segment crossing the Green Canal. The remainder of the road is a mostly uncurbed divided four lane roadway. The posted speed limit is 25 MPH. This road is a County north/south, collector roadway bisecting the northern half of Golden Gate M.S.T.U.
Signalized intersection:	Golden Gate Parkway and Green Blvd.
Curbing:	The median south of the Green canal are curbed with six inch ht. vertical type "D" concrete curb.
Median Description:	There exist .78 miles of median green area divided into six medians. The medians have landscaping and irrigation improvements. The medians range in width from 7 feet at turn lane locations to 18 feet at the widest locations.
Side Rights-of-Way:	The sides are grassed shoulders sloped into a drainage swale.
Roadway signage:	The roadway signage consist of metal traffic and road identification signs mounted on 4" x 4" wood post.
Overhead utilities:	There are no overhead electric transmission lines running along the roadway, but overhead lines do cross the roadway. Underground electric is present within some medians.
Roadway lighting:	There are no street lights continuously along the roadway, only at the intersections with other roadways. The street light fixtures are located in the medians and are double standard street lights mounted on top of a concrete pole. Decorative accent pedestrian lighting ls existing at the ends of all medians.
Pedestrian walkways:	There is 5 foot wide sidewalks along the east and west rights-of-way running from Golden Gate Parkway to 17 th Avenue S.W. From 17 th Avenue S.W. to Green Blvd. the sidewalk is only along the east side of the roadway. Pedestrian street crossings are marked.
Bike pathways:	Designated off-road, 5 foot wide facility shared with pedestrians.
Roadside Development:	The property along both sides of the roadway is developed with multi-family duplexes and apartments. The remaining properties are single family homes or vacant properties. The Golden Gate Community Center property is located at the N.W. corner with Golden Gate Parkway.
Traffic Calming:	In-road asphalt speed humps have been installed within the 20 th Place S.W. intersection at the school crossing.
Landscape Development:	Median landscape, irrigation and lighting improvements were completed in 2007.



EXISTING IMPROVED MEDIANS - SUNSHINE BLVD.

Tropicana Blvd. Existing Conditions
(Golden Gate Parkway, East to 32nd Ave. SW.)

Roadway descriptions:	A .80 mile roadway running north and south within road right-of-way easement. There are two-2 lane bridges over the Tropicana Canal and a bridge over the Golden Gate Canal for the entry into the high school. The road is a curbed divided two lane roadway. The posted speed limit is 25 MPH. This road is a County north / south collector roadway bisecting the southern half of Golden Gate M.S.T.U.
Signalized intersection:	Golden Gate Parkway
Curbing:	All medians are curbed with six inch vertical type "D" concrete curbing.
Median Description:	There exist .70 miles of median green area divided into six medians. The medians have landscaping, accent lighting and irrigation improvements. The medians range in width from 7 feet at turn lane locations to 18 feet at the widest locations.
Side Rights-of-Way:	The sides are grassed shoulders sloped into a drainage swale.
Roadway signage:	The roadway signage consist of metal traffic and road identification signs mounted on 4" x 4" wood post.
Overhead utilities:	There are no overhead electric transmission lines running along the roadway. Underground electric is present within medians supplying the lights.
Roadway lighting:	There are no street lights continuously along the roadway. The existing decorative light fixtures are located in the median ends
Pedestrian walkways:	There is 5 foot wide sidewalk along the east and west sides of the roadway approximately 3 feet off the pavement.
Bike pathways:	Two lane roadway in-road striped bike paths along both sides of the roadway.
Roadside Development:	The property along both sides of the roadway is single family or duplex residences. The remaining properties are undeveloped. Located on the northwest corner of the intersection with 32nd Ave. SW is a wastewater treatment facility.
Landscape Development:	Median landscape, irrigation and lighting improvements were completed in 2002.



EXISTING IMPROVED MEDIANS - TROPICANA BLVD.

**COLLIER COUNTY LANDSCAPE RELATED CODES, ORDINANCES,
MASTER PLANS, GUIDELINES AND STANDARDS**

Collier County Government has enacted landscape codes, ordinances, guidelines and standards related directly to providing minimum requirements for landscaping within and along County roadway corridors. See Section 8 for references and web site locations.

COLLIER COUNTY LANDSCAPE BEAUTIFICATION MASTER PLAN

The Master Plan was adopted on 04/22/2003 and addressed the landscape improvements and maintenance for County four and six lane arterial roadways as listed in the Master Plan. In May of 2009 the County Commission removed funding and updated standards for improvements, which affects the Golden Gate Beautification MSTU Master Plan roadways, Santa Barbara Blvd. (Golden Gate Canal to Green Blvd.) and Green Blvd. (Sunshine Blvd. to Santa Barbara Blvd). Collier County Landscape Beautification Master Plan changes are as follows:

Recommended Policy:

Collier County Landscape Master Plan requires Type "B" landscaping improvements on all 6-lane arterials within the urbanized area, budget permitting.

1. If a developer/MSTU/HOA desires to landscape a 6-lane roadway that has not been landscaped due to budget constraints and commits to improve the roadway to County established Type "B" landscaping standards, the developer/MSTU/HOA may do so at their own cost. The county would assume the maintenance responsibilities subject to Board approval. This condition would require the County to evaluate its ability to fund ongoing maintenance and, as such, final Board approval will be required for each request.
2. If a developer/MSTU/HOA desires to landscape a 4-lane highway depicted in Attachment A, which is either at its anticipated maximum lane configuration with no future plans for expansion, or expansion is beyond the 5-Year Capital Impact Element (CIE), and commits to improve the roadway to County established Type "B" landscaping standards, the developer/MSTU/HOA may do so at their own cost. If future 6-laning is identified in the Long Range Transportation Plan (LRTP), any landscaping must be installed in a manner not to conflict with the future expansion. The county would assume the maintenance responsibilities subject to Board approval. This condition would require the County to evaluate its ability to fund ongoing maintenance and, as such, final Board approval will be required for each request.

3. If a developer/MSTU/HOA wishes to improve County landscaping level through the installation of street trees (Type C-1) or through the installation of a buffer (Type C-2), the developer/MSTU/HOA would pay 100% of the additional plant material and irrigation improvements and be required to maintain the improvements since it exceeds the current approved levels of a Type B. This condition would require that the interested party enter into a landscape maintenance agreement with Collier County. The request and the landscape maintenance agreement would require Board approval for each request.

The northern portion of Santa Barbara Blvd. north of the Golden Gate Canal to the end of the existing curbed medians is ready for landscape improvements compliant with the Collier County Landscape Beautification Master Plan changes. The Golden Gate MSTU has the opportunity to develop a project to provide the landscape improvements for this six lane improved section of Santa Barbara Blvd. (Approx. 4,270 ln. ft., 8-medians with 73,669 sq. ft. of green area). The remainder of Santa Barbara Blvd. north from the end of the current existing six lane improved medians to Green Blvd. should be considered for median landscape improvements at a future date when the roadway improvements are finalized and the medians are fully curbed.

COLLIER COUNTY LANDSCAPE DEVELOPMENT CODE

Chapter 2.03.07 Corridor Management Overlay District

This Division affects the Golden Gate Parkway, West roadway corridor. The minimum requirements call for a 25 foot wide buffer area adjacent to the road right-of-way that retains 85 percent of the existing native vegetation. The trees planted shall be indigenous native species 8 foot in height and 30 feet on center. These requirements apply to all zoning uses except for single family homes in the Estates District.

Chapter 4.02.26 Golden Gate Parkway Professional Office Commercial Overlay District (GGPPOCO)

Special Conditions for the Properties Abutting Golden Gate Parkway East of Santa Barbara Boulevard as Referenced in the Golden Gate Parkway Professional Office Commercial District Map (Map 2) of the Golden Gate Area Master Plan.

Landscaping

1. Projects shall provide a ten (10) foot buffer between vehicular right-of-way and required sidewalk and shall provide landscaping of one (1) shade tree per thirty (30) linear feet. Such trees shall be a minimum of eight (8) feet in height and one and one-half (1½) inches in diameter at the time of planting that shall have a minimum canopy of

fifteen (15) feet at maturity. In addition, a hedge or berm planting combination shall be planted along the entire length of this ten (10) foot buffer consistent with section 4.06.00.

2. A minimum of ten (10) percent of the gross vehicular use area shall be landscaped to provide visual relief. One (1) tree, as described in section 4.02.26 I.1., shall be provided for each seventy-five (75) square feet of this landscaped area. This landscaping shall be placed within the vehicular use area.

Chapter 4.06, Landscaping, Buffering and Vegetation Retention

All commercial and multi-family projects abutting a roadway corridor must comply with the most current landscaping and buffering requirements.

Collier County Streetscape Master Plan

The Master plan addresses three arterial/collector roadways that pass through the M.S.T.U. District. The roadways are C.R. 951 (undefined specific locations between S.R.84 and Golden Gate Parkway), Golden Gate Parkway and Santa Barbara Blvd. The Master plan addresses all three roadways in a conceptual design manner, but only list C.R. 951 in its 5 or 15 year Streetscape Improvement Program.

Chapter 5.05.08 Architectural and Site Design Guidelines and Standards for Commercial Buildings and Projects, Landscaping.

The Land Development Code provides for additional buffer area widths adjacent to roadways, increases in tree heights to 12-14 feet and natural shaping of water retention areas with additional landscaping and features adjacent to the roadways.

Golden Gate Area Master Plan

This section of the Collier County Growth Management Plan has landscape requirements related to roadway buffering in two of the listed land use zoning categories. The "Golden Gate Professional Office Commercial District" requirements have now been adopted within the LDC Chapter 4.02.26. See above section for requirements. The second category is the "Golden Gate Estates Neighborhood Centers" which calls for a 25 foot wide buffer between the abutting right-of-way and the off-street parking area. This category only applies on the western corners of the intersection of C.R. 951, North and Pine Ridge Road Extension, East.

Public Rights-of-Way Construction Standards Ordinance 93-64

This ordinance specifies roadway construction standards and contains guidelines for roadway landscaping and irrigation. The

ordinances list requirements for all County arterial and collector roadways. The requirements cover plant setbacks, heights, size, types and clear sight distances. Sidewalk and bike path construction standards, including curb access with physically handicapped or impaired ramps are also specified within this ordinance. The ordinance also establishes the permitting procedure and requirements for installing landscaping within a road right-of-way.

Summary and Recommendations for County Codes

Based upon a review of the Land Development Code guidelines and standards it is apparent there exist some inconsistent and overlapping buffering and tree height requirements. It would be recommended that Chapter 4.06 and Chapter 2.03.07 requirements be combined so to provide for a 25 foot wide buffer with 12-14 foot height trees planted 30 foot on center be applied to adjacent properties along most of the area roadways within the Golden Gate Community Master Plan project area.

Developments along Golden Gate Parkway, East and C.R. 951, North to Green Boulevard should be exempted from the 25 foot buffer requirement due to the large amount of existing development. The existing Land Development Code Chapters 4.06 and 5.05.08 requirements will apply to these areas. The C.R. 951, Collier Blvd. corridor from Davis Blvd. northward is currently under planning for improvement to a six lane roadway.

The County landscape buffer requirements in place presently ensure for a landscape buffer area ranging from 10 to 25 feet in width be retained or created along the roadway of commercial or multi-family developed adjacent properties.

All the roadway corridor right-of-way easements within the project study area range in width from 106 to 130 feet. Based upon this the minimum landscape buffer width on adjacent properties throughout the project study area would be 15 feet.

As stated in the Collier County Streetscape Master plan methodology, out-lying communities with their own landscape programs are not included. It would be best to continue this exclusion and proceed with the existing M.S.T.U. District program. The exception to this exclusion is that the Beautification Advisory Committee, as well as the Golden Gate Community should public petition annually the Board of County Commissioners to have all the Golden Gate Community roadways added to the 5 and 15 year Collier County Streetscape Master Plan improvement programs to provide funding of roadway landscape improvements.

The existing County Land Development Code provides for penalties and fines for property owners who do not maintain the Code required landscape buffer plantings. A review of the roadways

within the project study area revealed that 50 percent or more of the developed properties required to install and maintain County Code landscape buffer plantings were not present or in a healthy condition as called for by the Codes. Due to the amount of work and limited personnel the County Code Enforcement Division has not been able to bring the problem under control and additional help is needed. It would be suggested that the Beautification Advisory Committee assist in this effort and write letters to the property owners in violation of the Codes and inform them of the Community beautification efforts, as well as offer assistance in the form of direction, knowledge or expertise.

The M.S.T.U. Advisory Committee on a continuous basis should monitor and be permitted to provide reviews of "Land Development Code" (LDC) Site Development Plan (SDP) submittals for projects located along the arterial and collector roadways within the M.S.T.U. district. These types of reviews can sometimes result in the development of a private/public partnership for roadway beautification.

Existing Pedestrian and Roadway Lighting

Pedestrian scale accent or safety lighting was virtually none existent prior to the implementation of this Master plan. Presently Tropicana Blvd. and Sunshine Blvd. have the pedestrian accent lighting installed on the median ends. The pedestrian accent lighting fixtures provide additional lighting for pedestrians and vehicles. The internal collector roadways of Coronado Parkway, Green Blvd., Hunter Blvd., Sunshine Blvd., and Tropicana Blvd. are not properly lit per the F.D.O.T. State standard of providing a 1.3 average foot candle level of light along the roadway.

These roadways only have streetlight twin fixtures within the medians located at intersecting roads. The light levels at these locations appear to meet the state standard. The only roadway that appears to have the proper light levels is C.R. 951, South, Part "B" from Golden Gate Parkway, East to Green Blvd.

Existing Pedestrian Walkways and Bike Pathways

C.R. 951, South, Part "B", Green Blvd., Golden Gate Parkway, Sunshine Blvd., Coronado Pkwy. and Tropicana Blvd. have asphalt and/or concrete, off-road and in-road shared facilities.

Community Gateway and Entry Roadway Signage

Existing Community Entry Signage

Located at the west end of the Golden Gate Parkway, East roadway is a Community entry signs that was part of the Golden Gate Parkway Beautification project. This sign was paid for and donated to the Beautification M.S.T.U by local organizations.

This type of public / private partnership should be encouraged for future signs. It would be recommended to adopt this existing sign format as the Community wide entry or boundary signage and incorporate this type of sign into other roadway beautification projects (See Figure 12).

Existing Roadway Traffic Control Signage

The Community roadway traffic signage is standard type painted metal signs mounted on galvanized poles or 4" x 4" wood posts. These signs include the stop signs, road name signs, speed limit signs and general traffic directional signs. This type of signage provides for no coherency or coordination to the community image.

Roadway Beautification Attitude Survey

A Golden Gate Community wide attitude survey about roadway beautification was developed and distributed to the community residents. A total of 34 responses were returned. The survey was an attempt to involve the community into the planning process. Even with the low number of responses the survey results provided information the Advisory Committee felt was important in their decision making process.



Community Entry Roadway Signage

TYPICAL BEAUTIFICATION PLAN CONCEPTS FOR ROADWAYS

The Advisory Committee during its public meetings decided earlier on that the Master Plan efforts would be directed to roadways located within the Beautification M.S.T.U. boundaries due to funding concerns. The other community roadways outside the M.S.T.U. as listed in the plan will be addressed in a written conceptual form. The Committee realized roadways outside of the District are important because of their inter Community connection as feeder roads to the Beautification M.S.T.U. District. Implementation of the following landscape concepts on uncurbed roadways will require approval from the Collier County Transportation Services Director.

The proposed landscape plan concepts were developed to provide for the most flexible means in which to perform roadway landscape beautification in regard to budgeting and phasing. These beautification landscape plan concepts were also developed in order to try and solve two major concerns associated with roadway landscape improvements. The first major concern is the County's requirement to install curbing around the median before landscaping is installed. The curbing serves two purposes. One is to act as a barrier to protect the plant material and irrigation sprinkler heads and the other is to stop undesignated median crossovers by motorists. The second major concern with the curbing is the cost of installation. Curbing costs are typically greater than 50 percent of the total cost of roadway landscape improvements. In an attempt to address these major concerns an undulating berm and retention area grading concept was developed. This grading concept would physically attempt to deter motorists from crossing the medians. The grading in conjunction with the plant material would visually deter the motorist to stay away and off the medians. This concept also provides an additional visual relief from the flat horizontal medians that exist throughout the Golden Gate Community roadways. The finished grade and plant placement on uncurbed medians must take into account the future installation of curbing as funds permit. Uncurbed medians with a width 14 feet or less will require an approval for canopy trees or palms to be place in the center of the medians.

The concept of planting trees along the side of the roads without outside curbed lanes can only occur if a special exception is granted by the Transportation Services Director per Ordinance 93-64. This is because of the set speed limit on the

roadways is over 30 MPH requiring a minimum tree setback along the right-of ways of 25 foot from the edge of pavement. If the trees were planted to this setback the trees would be on or beyond the right-of-way lines. All of the roadways within the M.S.T.U., except for Collier Blvd. (C.R.951), both the north and south sections, have no curbing on the outside lanes. The uncurbed median concepts are based on the medians not being irrigated or irrigated by a tanker.

Proper tree selection for tree plantings along or within the rights-of-way should be guided by Florida Power & Light's "Plant the RIGHT TREE in the RIGTH PLACE" south Florida edition.

UNCURBED 14 FEET OR WIDER AND CURBED 20 FEET OR WIDER MEDIANS

Typically most of the uncurbed median within the Master Plan roadways are 18 feet or greater in width, except at the turn lane locations. Curbed medians typically range from 15 to 38 (+/-) feet wide. Curbed medians wider than 20 feet have four inch height mountable type curb and medians under 20 feet have six inch height vertical face curbs. Proposed future curbing installed on medians 20 feet or less in width should be six inch vertical face F.D.O.T. Type "D" or Type "F" curb depending on the road drainage pattern.

Any of the following proposed typical type landscape planting plan concepts can also be implemented on roadways with existing curb around the medians and/or along the outside travel lanes.

Type T1 Typical Landscape Concept (Reference Drawing T1)

Type T1 is a median only installation of canopy trees or palms installed on a typical 50 foot center with shrubs beds planted at the base of the trees. The trees are to be placed in the grade transition area between the berm and retention area to help prevent motorist from crossing the median at location other than designated median openings. The Type T1 concept is basically considered a roadway median tree planting program.

Type T2 Typical Landscape Concept (Reference Drawing T2)

Type T2 is a combination of Type T1 with additional canopy and/or palm trees planted in the unpaved areas within and adjacent to the right-of-way lines per the County setback requirements. The trees shown along the sides are typically placed 50 feet on center and 10 to 20 feet off the edge of

pavement or 6 to 20 feet off the back of curbs. This Type T2 concept is an entire roadway tree planting program.

Type T3 Typical Landscape Concept (Reference Drawing T3)

Type T3 is a median only planting plan. It utilizes low growing ground covers or turf on top of the berms and at the median ends medium height shrubs within the retention areas. The plant material located within the retention or low areas should be wetland or water tolerant type plants and the plants on top of the berms should be upland or more drought tolerant.

Type T4 Typical Landscape Concept (Reference Drawing T4)

Type T4 is a combination of Type T3 with additional canopy and/or palm trees planted in the unpaved areas within and adjacent to the right-of-way lines per the County setback requirements. The trees shown along the sides are typically placed 50 feet on center and 10 to 20 feet off the edge of pavement or 6 to 20 feet off the back of curbs.

CURBED MEDIANS 8 TO 20 FOOT WIDTH

The following landscape concepts are not cost effective in medians wider than 20 feet and should be only implemented in the center of the medians on roads expected to receive additional traffic lanes.

Type T5 Typical Landscape Concept (Reference Drawing T5)

Type T5 is a median only planting with no turf areas.

Type T6 (Reference Drawing T6)

Type T6 is a combination of Type T5 with additional canopy and/or palm trees planted in the unpaved areas within and adjacent to the right-of-way lines per the County setback requirements. The trees shown along the sides are typically placed 50 feet on center and 10 to 20 feet off the edge of pavement or 6 to 20 feet off the back of curbs.

CURBED 8 TO 6 FOOT OR LESS MEDIANS OR SEPARATORS

Curbed medians 8 to 6 feet or less in width or typically medians at turn lane locations should be paved with decorative interlocking pavers. This would also include the existing solid concrete separator medians at turn lane locations.

UNCURBED ROADS: IN-ROAD PATHWAYS FACILITY CONCEPTS

In an attempt to reduce the overall landscape installation and maintenance cost, as well as improve pedestrian/bicycle pathway facilities, the following two in-road pathway concepts are recommended. The first concept is the "Four lane road in-road Facility" concept requiring a two foot wide strip of pavement be added to the inside lanes reducing the median width to 14 foot (+/-). The road pavement would then be restriped to provide for two 10 foot traffic lanes and a 4.5 foot wide in-road pathway facility on each side (See Figure 13). The second concept is a "Two lane road in-road Facility" concept requiring the road to be restriped to one 14 foot traffic lane and one 8 foot wide in-road pathway facilities on each side (See Figure 14). With both these concepts an additional pedestrian path on one side of the roadway would be recommended to provide for the safest pedestrian circulation. The overall maintenance of these facilities as listed above should be less from both a repair and landscape cost point of view.

The following M.S.T.U. internal collector roadways would be recommended for the "Two lane road in-road Facility" concept:

Roadway Name

Hunter Boulevard
Sunshine Boulevard
Tropicana Boulevard

and the following County major arterial roadways for the "Four lane road in-road Facility" concept:

Coronado Parkway
Santa Barbara Boulevard, North

RECOMMENDED BEAUTIFICATION CONCEPTS FOR THE BEAUTIFICATION M.S.T.U. ARTERIAL/COLLECTOR ROADWAYS

1. Coronado Parkway

Landscape: Type T1 or T5, T5 to be installed in 2011.
Lighting: Pedestrian fixtures at main intersections or pedestrian crossings. Install twin headed decorative streetlight fixture within the medians on 200 (+/-) foot centers.
Signage: Install decorative street name and traffic control signs.
Pathways: Four lane road in-road Facility concept along with existing 5 foot wide off-road

facilities located along the south and north rights-of-way (See Figure 13).

2. Collier Blvd. (C.R. 951), South

Part "A" (Golden Gate Canal to Golden Gate Parkway)

Landscape: Type T1, T5 or T6, Type T5 Completed
Lighting: Pedestrian fixtures at main intersections or pedestrian crossings. Install decorative streetlight fixture on both sides of the roadway spaced triangularly on 200(+/-) foot centers.
Signage: Install a Community "Welcome to Golden Gate" entry sign in the west right-of-way area just north of the Golden Gate Canal bridge. Install decorative street name and traffic control signs. Completed
Pathways: 8 foot wide off-road facility along west right-of-way area.

Part "B" (Golden Gate Parkway to Green Boulevard)

Landscape: Type T1, T3 or T4, Type T3/T5 combined, Project Completed.
Lighting: Pedestrian fixtures at main intersections or pedestrian crossings and replace existing streetlight fixtures with decorative poles and fixtures.
Signage: Install a Community "Welcome to Golden Gate" entry sign on the N.W. corner of Green Blvd. Install decorative street name and traffic control signs. Completed
Pathways: Existing facility

3. Golden Gate Parkway, East

Landscape: Existing, Type T3/T5 combined, Completed
Lighting: Pedestrian fixtures at main intersections or pedestrian crossings. Install decorative streetlight fixture on both sides of the roadway spaced triangularly on 200 foot(+/-) centers
Signage: Install decorative street name and traffic control signs. Community "Welcome to Golden Gate" entry sign on west end. Completed
Pathways: Install an off-road 5 foot wide facility along the south right-of-way area to make a continuous connection along the roadway. Project Completed.

4. Green Boulevard

Landscape: Type T1 or T6 on the eastern four lane section and continue the side right-of-way areas trees along the western two lane section.

Lighting: Pedestrian fixtures at main intersections or pedestrian crossings. Decorative twin headed streetlight fixtures within the medians on the eastern half and along the southern edge of pavement on the western half spaced 200(+/-) foot on center.

Signage: Install decorative street name and traffic control signs.

Pathways: Existing facility

5. Hunter Boulevard

Landscape: Type T1 or T5, T5 to be installed in 2011.

Lighting: Pedestrian fixtures at main intersections or pedestrian crossings. Decorative twin headed fixtures as specified in the Master Plan within the medians spaced 200(+/-) foot on center.

Signage: Install decorative street name and traffic control signs.

Pathways: Two lane road in-road Facility concept along with a 5 foot wide pedestrian facility along the southwestern right-of-way.
(See Figure 14). Project Completed.

6. Santa Barbara Boulevard, North

Landscape: Type T1, T5 or T6

Lighting: Pedestrian fixtures at main intersections or pedestrian crossings. Install decorative streetlight fixture on both sides of the roadway spaced triangularly on 200(+/-) foot centers.

Signage: Install decorative street name and traffic control signs.

Pathways: Install a 5 foot wide off-road facility along both sides of the roadway.

7. Sunshine Boulevard

Landscape: Type T1 or T5, Type T3/T5 combined, Project Completed.

Lighting: Pedestrian fixtures at main intersections or pedestrian crossings. Decorative twin headed fixtures within the medians spaced 200(+/-)

foot on center. Pedestrian fixtures at median ends completed.

Signage: Install decorative street name and traffic control signs.

Pathways: Two lane road in-road Facility concept along with existing off-road pedestrian facility along the east right-of-way (See Figure 14).

8. Tropicana Boulevard

Landscape: Type T1 or T5, Type T3/T5 combined, Project Completed.

Lighting: Pedestrian fixtures at main intersections or pedestrian crossings. Decorative twin headed fixtures within the medians spaced 200(+/-) foot on center. Pedestrian fixtures at median ends completed.

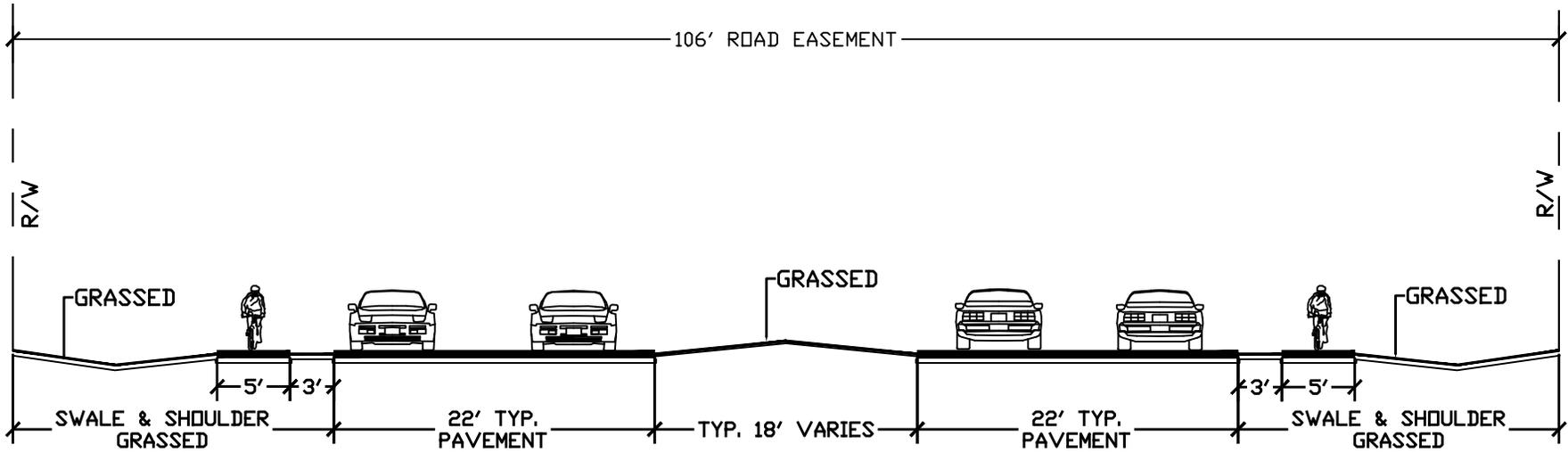
Signage: Install decorative street name and traffic control signs.

Pathways: Two lane road in-road Facility concept along with existing off-road pedestrian facility along the west right-of-way (See Figure 14). Project Completed.

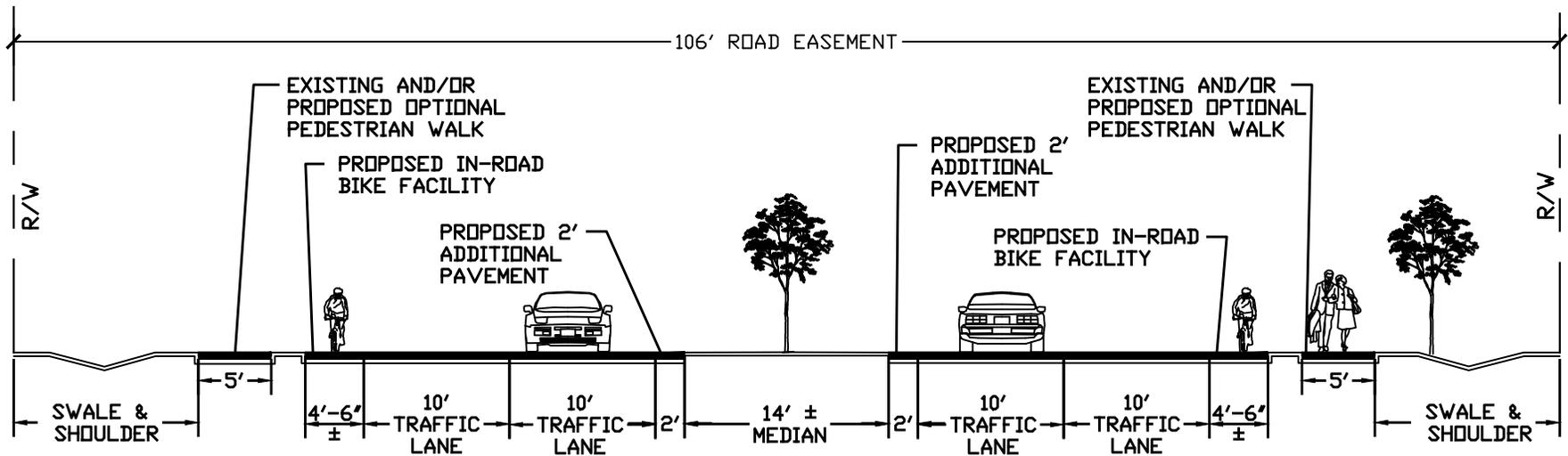
Summary

The Beautification M.S.T.U. District roadways numbers one through eight landscape concepts were selected based on low annual landscape maintenance cost. The selection will provide, upon completion of the landscape improvements, the estimated lowest annual total landscape maintenance costs. The Type T1 and T5 landscape planting concepts will require the least annual landscape maintenance costs.

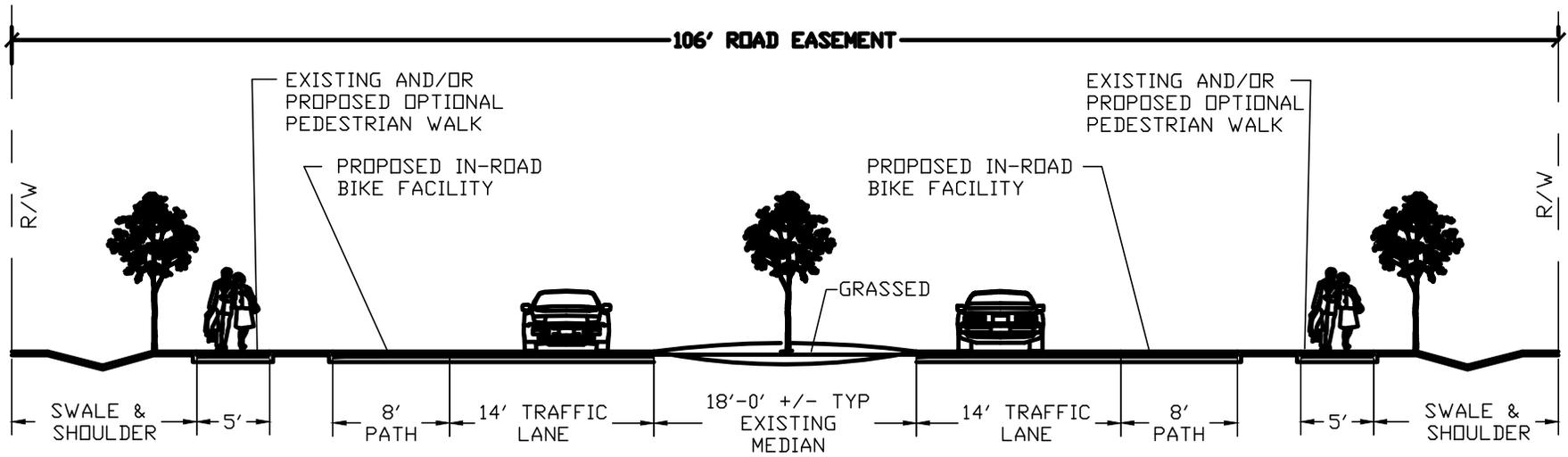
UNCURBED ROADS IN ROAD PATHWAYS PAVEMENT CONCEPT



TYPICAL EXISTING



FOUR LANE ROAD IN ROAD FACILITY



TWO LANE ROAD IN-ROAD FACILITY

ROADWAY BEAUTIFICATION
PLAN ELEMENTS AND DESIGN STANDARDS

The goal of these plan elements and design standards is to provide for a feasible, economical, safe, more pedestrian friendly and aesthetically pleasing roadway system for the residents and motorists. These elements and standards are to be guides for preparing the final design and installation plans.

The Beautification M.S.T.U. Advisory Committee and the Collier County Government will need to continue in their public/private partnership in the planning, funding, design and maintenance of future M.S.T.U. roadway beautification projects. It is recommended that in the final design phase of future projects that a Florida Registered Landscape Architect be involved in the process.

The following will be a list of items to be considered and/or incorporated into final design plans:

Community colors

The community colors are to be forest green and cream. The forest green is to be equivalent to Tiger Drylac® powder coating color Ral 6016 and the cream equivalent to Sherman Williams exterior Gentry cream SW2332. The community colors are to be utilized singularly or in combination on site furnishing and fixtures such as benches, traffic control signage and decorative light fixtures.

Sight Distance

For required sight distances for landscaping in medians at median crossover locations and at roadway intersections (See F.D.O.T. "Design Standards" Index 546). For roadways with posted 25 MPH speeds refer to F.D.O.T. "Manual of Minimum standards for Design, Construction and Maintenance for Streets and Highways" as a reference standard.

Sight Window

For required vertical sight window areas that must be kept clear of foliage that can block a motorists view (See F.D.O.T. "Design Standards" Index 546).

Pedestrian / Bicycle Pathway Facilities

On many of the M.S.T.U. roadways, the pathway facilities are non-existent along both sides of the roadway or are not continuous

along the roadways. Refer to the Pedestrian/Bicycle Facilities Map for location of existing and proposed facilities within the M.S.T.U. roadway corridors.

The installation of proposed pathway facilities should be planned and be included in future roadway beautification projects. Coordination with the Naples (Collier County) Metropolitan Planning Organization Pathways Work Program and Advisory Committee will provide assistance and possible funding for pathways.

Pathways Facility Design Standards:

Due to the large amount of lineal feet and overall lack of facilities within the M.S.T.U. roadways it is recommended to use concrete facilities. The implementation of the "In-road Pathways Pavement Concepts" would shorten the time frame of installation. Installation should be incorporated, budgeted and scheduled with Collier County road resurfacing plans and schedules.

The minimum width of a concrete one way pedestrian/bicycle facility should be 5 feet. The minimum width of a two way concrete facility should be 8 feet.

Pedestrian Crosswalk Marking

Pedestrian crosswalk locations should be marked. Refer to the "Manual on Uniform Traffic Control Devices for Streets and Highways" current edition for crosswalk markings. Crosswalk markings with longitudinal lines is recommended for added visibility. Surface applied reflective white brick pattern surfacing should be considered as an alternate to painted white bars. Sidewalk curb ramps at all streets or large commercial driveways crossings shall have embedded domed warning surfaces per F.D.O.T. "Roadway and Traffic Design Standards" Indexes. Warning surfaces shall be a contrasting earth tone (i.e. clay brick) color to the paved surface.

Curbing

Median curbing shall be concrete (6) six inch vertical face Type "F" or "D" curb per F.D.O.T "Roadway and Traffic Design Standards" Index #300, most current edition.

Decorative Paving

Medians 6 feet or under in width, concrete separators, and medians adjacent to turn lane stack areas shall be paved with decorative concrete interlocking pavers (See Figure 16). The paver pattern shall simulate a cobblestone pattern made up of

a mix of three different paver sizes and colors. The colors shall be dark emerald green, medium green and cream. The percentage ratio for each paver color shall be one third. Existing installations should be reviewed in order to match colors.

Roadway and Pedestrian Decorative Lighting

Roadway: There shall be two types of roadway/street lighting recommended for the M.S.T.U.

Golden Gate Master Plan "Special Overlay District"

Pole & Fixture: Collier County Traffic Operations
"East Trail Street Light Assembly"
(Lumec 250 watt metal halide, DMS30
LMS11684A, Type A), Drawing No. CCTO 03-204

M.S.T.U. Roadways

Pole: Green toned exposed aggregate round pole anchor based or embedded, topped with a single or double 6 foot fixture arm, color forest Green.

Fixture: Lumec Helios series H.B.M., forest green Color, or approved equal.

F.D.O.T. minimum average light levels of 1.5 initial foot candles along the road shall be maintained.

The pedestrian lighting shall be a decorative pedestrian scaled pole and fixture. The color shall be one or a combination of the community colors. These fixtures are to be located at major intersection corners, median ends and pedestrian crosswalks. These fixtures are to provide daytime accent and additional night time safety lighting.

Decorative Pedestrian Accent Lighting

Pole: Lumec, Inc. AM6U-16-GN6-TX 16 foot ht. pole with base cover or approved equal.

Arm: VR 302-1A-GN6-TX

Fixture: Lumec, Inc #175SMH-DMS50-SG3-LD-QTA240 or approved equal.

Soil Analysis and Preparation

Prior to planning and design of any future projects it is recommended that test borings be performed in the road medians. The existing soil within the medians shall be removed to a depth of 8 to 12 inches minimum; or to a depth to remove any limestone or construction debris. The areas shall then be filled with a specified soil mix or with a local clean sandy loam top soil. The ph level of the soil should be within the ranges of 5.5 to 6.8.

If local top soils are used or existing soil is to be amended, it is recommended that 25 to 50 percent by volume of sewage sludge be incorporated into the soil. The Collier County Wastewater Treatment Facilities produce, and can provide, the sewage sludge. Water management polymers and wetting agents should be incorporated in the soil per Manufacturers specifications.

Site Furnishings and Fixtures

- Benches: Wausau Tile™ Model TF5047 with cream weatherstone concrete supports and forest green colored recycled plastic seats and backing or approved equal.
- Trash receptacles: Wausau Tile™ precast Model A, TF1205, cream weatherstone finish with forest green plastic arch lid or approved equal.
- Bike Rack: Six cycle tubular loop rack with in-ground mount. The color shall be forest green.

Street Identity and Traffic Control Signage

Typical existing standard metal roadway street name or traffic control signs shall be mounted on a wood backing and then framed and mounted on a decorative post system. The posts and frames shall be cream colored and the bands and sign backing or highlighted objects shall be forest green. The support posts for all stop signs, large traffic control or informational and street identity signs or combination of shall be a 6" x 6" square post (See Figure 15).

Landscape Sight Distance and Sight Windows

The landscape plantings must comply with the sight distance and sight window details (See Figures 18A and 18B). These details graphically depict the clear sight requirements as set forth in 1997 edition of Collier County Ordinance 93-64 and the "Construction Standards Handbook for Work within the Public Right-of-Way" and as detailed in this plan. The details establish minimum tree setbacks, clear sight zones, and plant and canopy height requirements. Limit of clear sight zones shall be increased or adjusted at horizontal curve locations so to provide for the greatest clear sight zone possible.

Major Intersections

Special landscape improvements should be implemented at major intersections in order to establish a pronounced entry into the Community. Major intersections would be defined as signalized or

un-signalized intersections where two major County arterial roadways intersect. The intersections within the M.S.T.U. which would be considered major are the Golden Gate Parkway, East and Santa Barbara Blvd., North intersection, Collier Blvd. (C.R. 951) at Green Blvd. and Golden Gate Parkway intersection and the Santa Barbara Blvd., North and Green Blvd. intersection.

Major intersections should receive the following type improvements:

1. International pedestrian crosswalk markings of solid white bars.
2. Decorative street and pedestrian lighting fixtures.
3. Decorative street name, traffic control and Community entry signage.
4. Decorative mast arm traffic signal control pole systems
5. Site furnishing: Benches, trash containers and bike racks.
6. Pedestrian safe areas in medians and at corners delineated with decorative paving.

The Golden Gate Parkway and Santa Barbara Blvd., intersection is listed in the County approved 1988 "Corridor Management Study" as a major intersection per the study.

LANDSCAPE DESIGN STANDARDS AND GUIDELINES

The intent of these landscape design standards are to provide for the safety of motorists and pedestrians while creating a thriving urban landscape within the M.S.T.U. roadways. The general planting concept for all roadways will be to utilize native plant species and naturalized accent plants to try and restore a planting image based upon the upland pine forest vegetation community, (Refer to Drawings T1 through T6 Appendix A). Roadway streetscape improvements will be guided by the "Collier County Landscape and Irrigation Specifications for Beautification Improvements within the Public Right-of-way." most current edition. A pre-design meeting and an on-site review with the Collier County Transportation staff is recommended.

The median widths listed below refer to the inside of curbing or planting area.

Curbed 8 to 6 feet wide or less medians/separators

These type medians shall be paved with decorative interlocking pavers as specified and as funding permits per "Curbed Turn Lane & Decorative Paving Detail" (See Figure 16).

Uncurbed 8 to 6 feet wide or less medians

These medians shall be bermed per "Uncurbed Turn Lane & Berm

Detail" (See Figure 17).

Curbed 20 to 8 foot wide medians

These type medians shall be landscaped with plants and/or decorative pavers only. No turf grasses.

Uncurbed 14 feet or wider or curbed 20 feet or wider medians

These type medians will typically contain turf and shall be landscaped per the "Typical Roadway Landscape Plans" Type T1, T2, T3 or T4, (Refer to drawings T1 through T4 Appendix A) and "Uncurbed Turn Lane & Berm Detail", (See Figure 17).

Existing or Proposed Curbed Roadways

Collier Blvd. (C.R. 951) South, Part "A" **(G.G. Canal to G.G. Pkwy.)**

Typical landscape plan: Existing Type T5, medians planted with no turf.

Irrigation: Existing well & pump with conventional pop-up system.

Curbing: Existing vertical face curb around medians and along outside lanes.

This segment of roadway should be treated as a major entry or gateway road into the community. The planting design should have a mature and dramatic appearance to create an entrance.

Collier Blvd. (C.R. 951) South, Part "B" **(G.G. Pkwy. to Green Blvd.)**

Typical landscape plan: Existing Type T3, center of medians planted with 18' wide beds spaced between 100 and 200 foot on center. Unplanted median areas proposed for future traffic or turn lanes shall be grassed.

Irrigation: Existing well & pump with conventional pop-up system.

Curbing: Existing mountable and vertical curb around medians and vertical along outside lanes.

This segment of roadway is totally zoned and 90% developed with roadside commercial facilities. Attention must be given in the landscape design to address visibility for the high amount of automobile traffic entering and exiting this roadway.

Coronado Parkway

Typical landscape plan: Type T5, medians with plant beds.
Irrigation: Conventional pop-up system.
Curbing: Type "F" vertical face 6 inch ht. curb.

Hunter Boulevard

Typical landscape plan: Type T5, medians with plant beds.
Irrigation: Conventional pop-up system.
Curbing: Type "F" vertical face 6 inch ht. curb.

Sunshine Boulevard

Typical landscape plan: Existing Type T3, medians with plant beds and turf.
Irrigation: Existing well & pump with conventional pop-up system.
Curbing: Existing vertical face 6 inch ht. curb around median noses and turn lanes.

Tropicana Boulevard

Typical landscape plan: Existing Type T3, medians with plant beds and turf.
Irrigation: Existing well & pump with conventional pop-up system.
Curbing: Existing vertical face 6 inch ht. curb around median noses and turn lanes.

**Santa Barbara Boulevard, North
(Coronado Pkwy. to Green Blvd., Phase II)**

Typical landscape plan: Type T5, medians with plants and no turf.
Irrigation: Conventional pop-up system.
Curbing: Vertical face curb around entire medians.
Decorative paving: Within turn lane stack areas and/or separators.

Santa Barbara Boulevard, North
(G.G. Canal to end of existing 6-lane Roadway Improvements, Phase I)

Typical landscape plan: Type T5, medians with plants and no turf.
Irrigation: Conventional pop-up system. Irrigation sleeves are existing.
Curbing: Existing.
Decorative paving: Existing.

Golden Gate Parkway East

This segment of roadway has been irrigated and landscaped. The landscape design used was a modified Type T3 as listed above. Continued maintenance and renovation of plant beds will be needed.

Existing Uncurbed Roadways

Green Boulevard

Typical landscape plan: Type T5, medians with plant beds.
Irrigation: Conventional pop-up system.
Curbing: Existing 6" ht. curb around median noses and Type "F" vertical face 6 inch ht. curb.

IRRIGATION WATER RESOURCES AND IRRIGATION SYSTEMS

The following information evaluates the potential use and cost effectiveness of water resources and irrigation systems that would be available for use within the Beautification M.S.T.U. area.

Reclaimed water transmission line systems

Two reclaimed water transmission line systems were analyzed. The systems would provide reclaimed water for all roadways within the M.S.T.U. boundary. One system utilized the Florida Cities Utilities Company's existing wastewater treatment plant as the source. The high estimated costs to install this system made the implementation unfeasible. The other alternate system was a

reclaimed water transmission line system connected and supplied from the proposed Collier County reclaimed water pipe line that crosses at the intersection of Collier Blvd. (C.R. 951) and Vanderbilt Beach Road Extension. The high estimated costs for installing this system made the implementation unfeasible. The costs of both systems are unrealistic based upon present and projected M.S.T.U. revenues.

Reclaimed water tanker applied

This method of watering would not require in place irrigation system equipment or components. The water would be applied with a special equipped tanker truck on medians with a width of 20 feet or less. This type of watering method is a very successful and cost effective system for watering landscape areas where the potable or treated water cost is high or where a well and pump system is not feasible such as in coastal areas. This type method of irrigating is presently being utilized within the City of Marco Island on its un-irrigated roadway medians. This method of water because of its high annual operational costs would use to much of the M.S.T.U. operational revenues at build out.

Well and pump transmission line system

This study was based on installing an inter connecting well and pump system to all roadway areas. The system would utilize the existing wells, pumps and transmission lines located along Golden Gate Parkway East. The estimated high cost to implement this system with the present M.S.T.U. revenues makes it unrealistic.

Potable / Treated water

The internal roadways of Coronado, Sunshine, and Tropicana could be connected to the existing potable water system. The initial connection costs would be very low, but the long term use cost per gallon would be very high.

Conventional pop-up spray systems

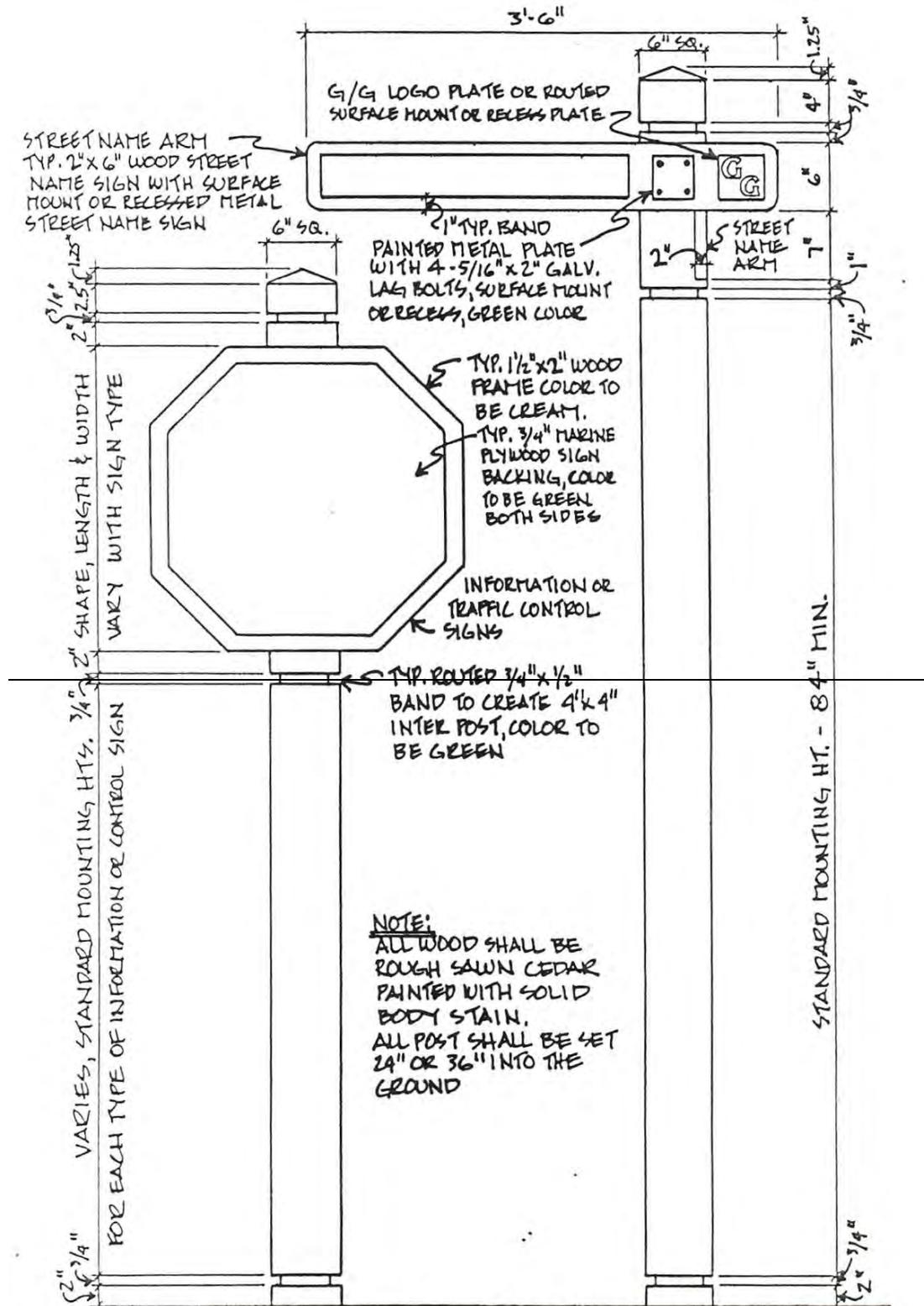
The conventional irrigation system remains the most cost effective system to install and maintain. The system shall be designed so as not to permit or reduce to the greatest extent possible, overspray and seeping onto the paved areas. The pop-up systems must utilize low volume, low angle spray nozzles and pressure reducing devices. Systems designed for medians using pop-up mist type spray sprinklers should have the sprinklers spaced on a triangular layout verses a square layout whenever possible. This will reduce the total number of sprinklers required in the medians. Systems designed for medians with turf using pop-up rotor sprinklers should be designed so the

sprinklers are spaced on a square head to head layout and installed 18 inches off back of curbing. In medians wider than 20 feet with a generally centerline planting of drought tolerant plants should have head to head coverage along the sides of the medians and not necessarily across the median width. The centerline median planting of drought tolerant plants naturally requires less water. Medians 20 feet or wider with just turf should have an additional center row of sprinklers in order to reduce the sprinkler radius requirements. The above pop-up sprinkler layouts combined with utilizing drought tolerant plants provides for water coverage while minimizing the chance of overspray and seeping. All sprinklers within the roadways should be installed on flexible plastic pipe off the lateral piping.

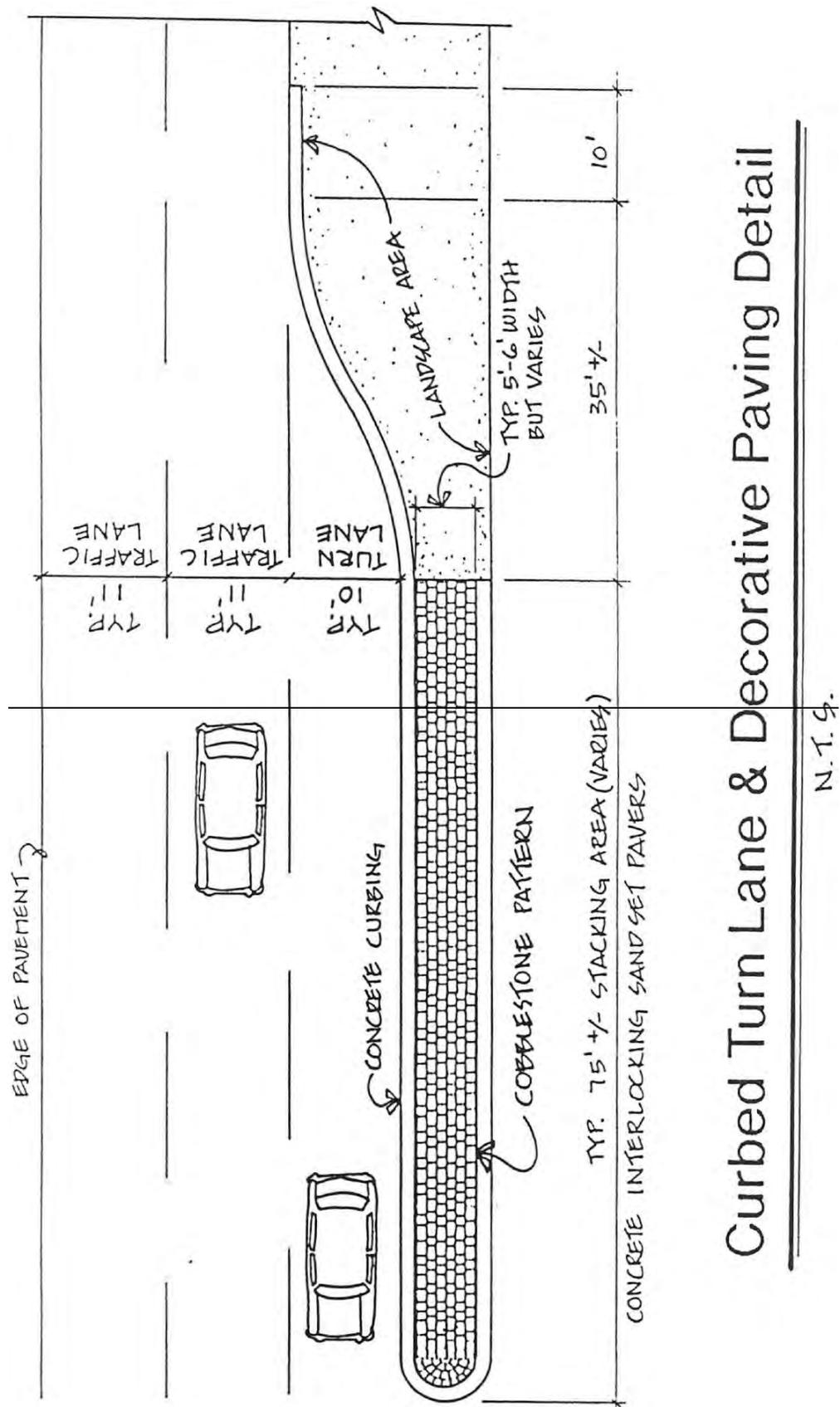
Irrigation zoning should occur based upon plant type water requirements whenever possible. Canopy trees and palms should be on a separate low volume drip/bubbler zone.

IRRIGATION, PLANT MATERIAL AND MAINTENANCE PLANS AND SPECIFICATIONS

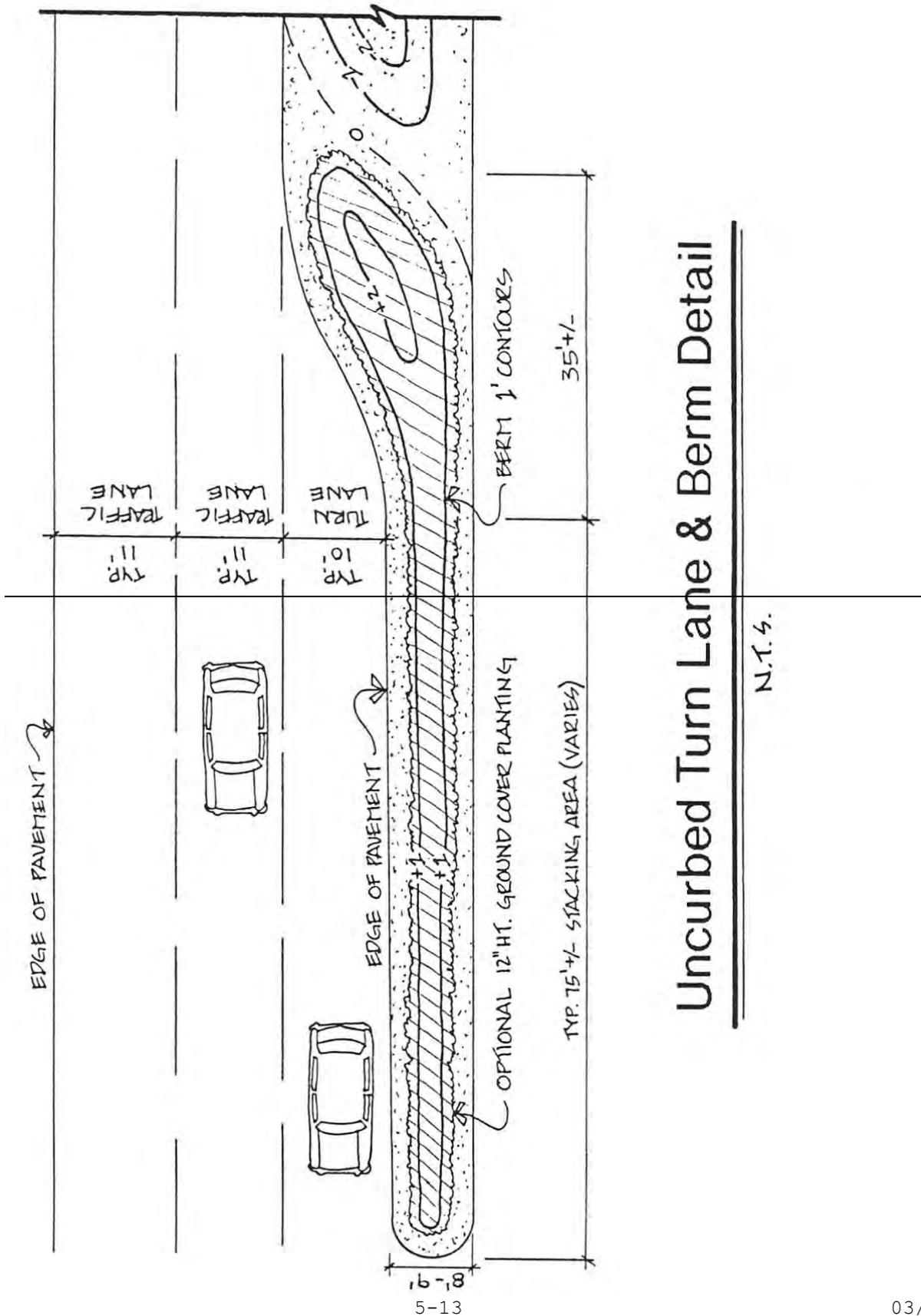
Refer to the Collier County Right-of-way Ordinance "Construction Standards Handbook for Work within the Public Rights-of-Way" most current edition for irrigation, plant materials and maintenance standards and guidelines.

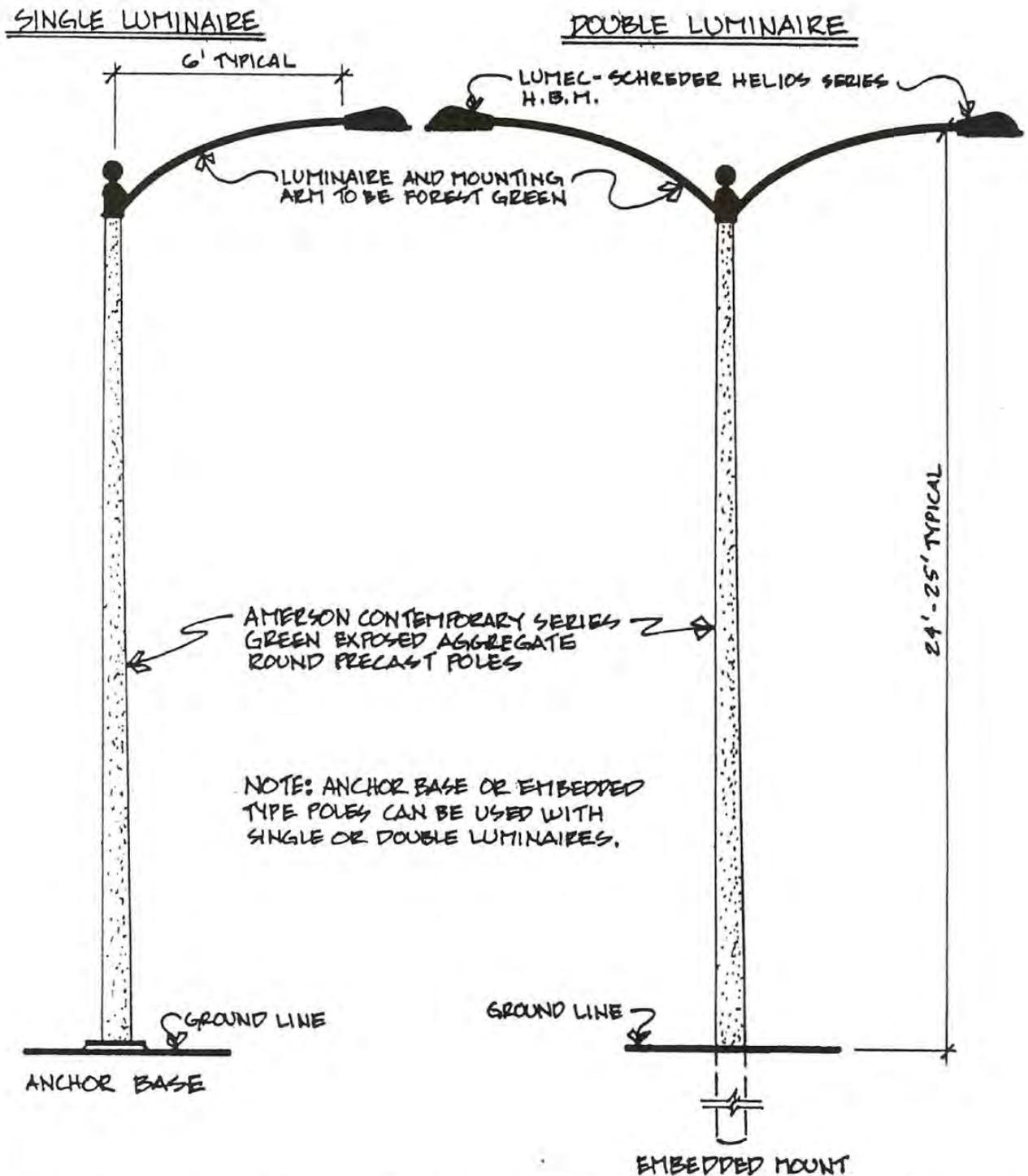


Street Name & Traffic Control Signage

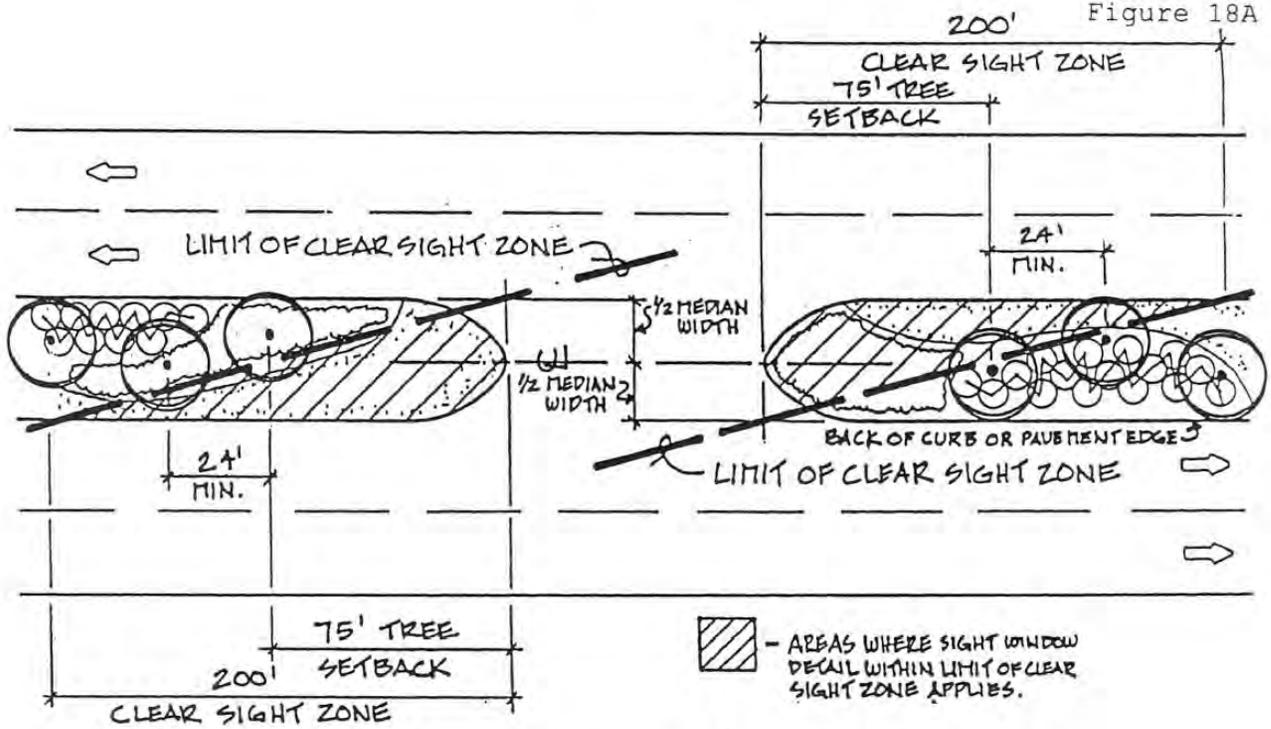


Curbed Turn Lane & Decorative Paving Detail



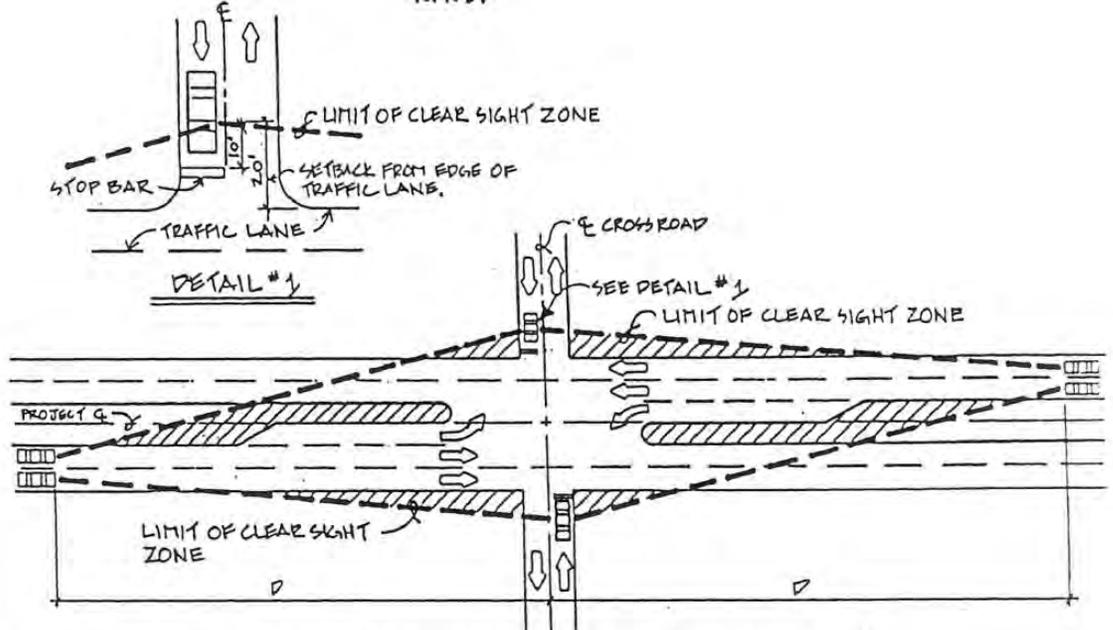


Street/Roadway Decorative Light Fixtures



TYPICAL MEDIAN CROSSOVER SIGHT DISTANCE

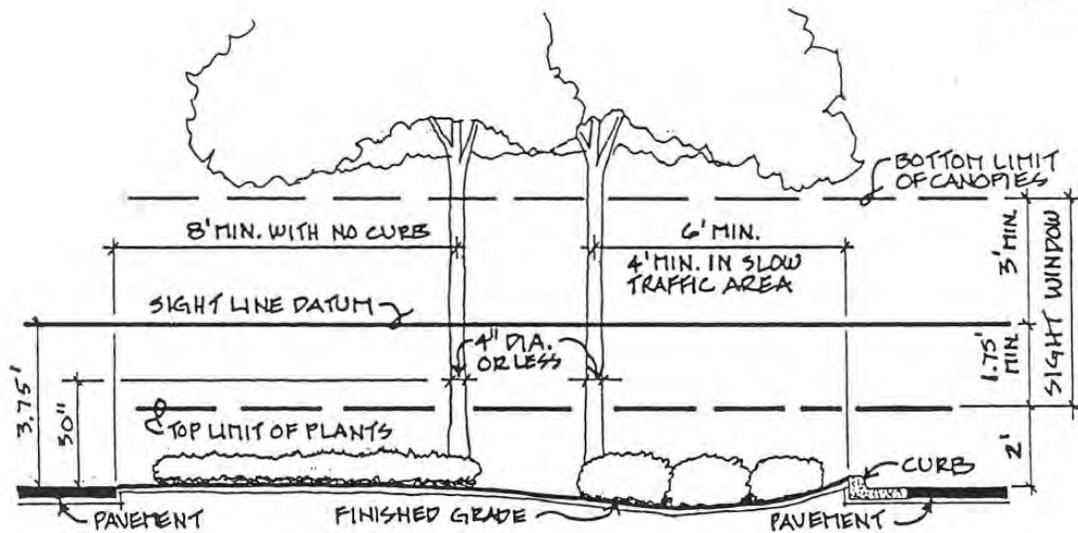
N.T.S.



POSTED SPEED (MPH)	35	45	55
SIGHT DISTANCE (D) FT.	D	D	D
MINIMUM	250	300	450
DESIRABLE	300	400	550

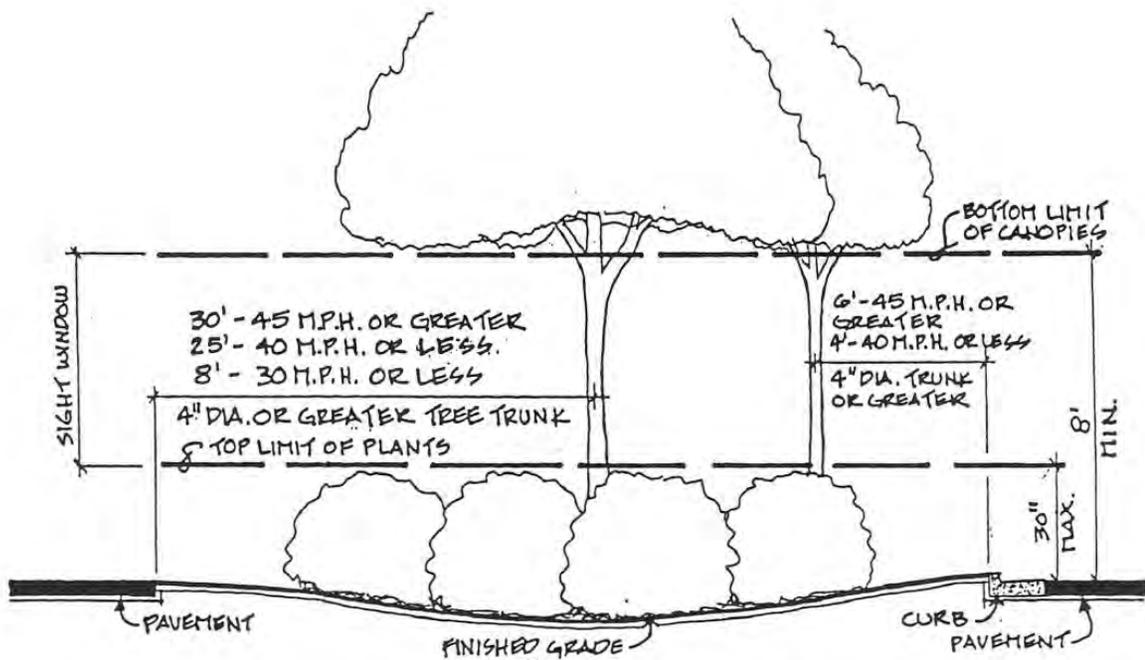
SIGHT DISTANCE DETAIL FOR MULTI LANE DIVIDED ROADWAY

N.T.S.



Sight Window Detail within Clear Sight Zone

N.T.S.



Typical Sight Window Detail

N.T.S.

BEAUTIFICATION IMPROVEMENT COSTS AND CONSIDERATIONS

The cost information utilized in these estimates are based upon Collier County Government and F.D.O.T. statewide average bids and existing contract pricing related to roadway landscape, curbing, irrigation and lighting improvements.

Planting cost

The plant material costs were based upon the following sizes:

Canopy trees:	10'- 12' ht., 2.5" cal., 50'o.c.
Accent trees:	8'-10' ht., 30 gal. or B & B
Cabbage palms:	10'- 15' ht. clear or booted trunks
Large shrubs:	10 gal. 4'-5' ht.
Small shrubs:	3 gal. 18"-24" ht.
Ground covers:	1 gal. 6"-12" ht.

Irrigation System

The irrigation costs include an installed 100 percent coverage, conventional pop-up system. The cost covers casing/sleeves, main lines, wiring, valves, sprinkler heads and pump & well systems.

Design and Contract Administration Fees

These fees are for professional design services and/or consultation from a registered landscape architect and registered engineer for lighting projects.

Site Preparation

Covers the costs for vegetation removal, grading, soil removal and/or replacement.

Concrete Curbing

Cost to install concrete curbing around entire median. Curbing cost include minor modifications to the turn lane tapers and median opening closures.

Decorative Paving

Covers cost to install sand set concrete interlocking pavers.

Two lane Road In-Road Pathway Facility

The cost shown under this item applies to implementing the bike facilities concept based on the "Uncurbed Collector Roadways Development Concept" section/elevations. In-road facility cost only includes cost of re-stripping roadway.

Pedestrian Decorative Lights

Cost to install decorative pedestrian scale lighting at median ends and at major pedestrian oriented intersections or crossings.

Annual Landscape Maintenance

These costs are based upon current contract pricing and cover services as specified under the technical maintenance specifications being presently implemented by Collier County Transportation Services.

Asphalt Paving

Cost to install asphalt paving for drainage improvement, turn lane extensions and patching purposes.

Pedestrian Crosswalk Markings

Cost to install surface applied high visibility pedestrian crosswalk markings.

BEAUTIFICATION IMPROVEMENT OPINION OF COSTS AND CONSIDERATIONS FOR ROADWAYS

CORONADO PARKWAY, ROADWAY #1

(Golden Gate Parkway, East to Santa Barbara Blvd., North)

Roadway length .91 mile, Medians green area length .74 mile, (3,268 l.f.)

Total median area – 38,048 s.f., Total right-of-way maintenance area - 181,500 s.f.

<u>Project Components</u>	<u>FY 10/11 Coronado Pkwy. & Hunter Blvd. Combined Bid/Contract Pricing</u>
Design & Administration:	\$ 47,691.00 (Landscape, Engineering, Surveying & Lighting Design)
<u>Phase I Site Preparation & Curbing</u>	\$ 344,243.00
Site Preparation and Soil Backfill:	\$ <u>Included</u> (Veg. removal, grading & topsoil)
Concrete Curbing:	\$ <u>Included</u> (FDOT Type "F" curb)
Decorative Paving:	\$ <u>Included</u> (Sand set Concrete pavers)
Two Lane In-road Pedestrian Facility	\$ <u>NIC</u> (Re-striping existing asphalt)
Pedestrian Crosswalk Markings:	\$ <u>Included</u> (Surface applied reflective markings)
Asphalt Paving:	\$ <u>Included</u> (Selective paving for drainage)
Pedestrian Accent Lighting:	\$ <u>NIC</u> (Fixtures purchased directly)
Decorative Roadway Lighting:	\$ <u>NIC</u> (Golden Gate Master Plan Fixtures)
Sleeving:	\$ <u>Included</u> (Irrigation & Electrical sleeves)
Landscape Plantings:	\$ 130,896.00
Irrigation System:	\$ <u>Included</u> (Well & pump station)
Sub-Total:	\$ 522,830.00
Contingency (10%):	\$ <u>-0-</u> (or as required by County Policy)
Project Budget Total:	\$ 522,830.00
 <u>Annual Maintenance</u>	 \$ 25,000.00 (Estimated)

- Note:**
1. The above costs are based upon actual FY 10 project construction bid and contract pricing.
 2. Maintenance costs derived from FY 09/10 County maintenance bid pricing.
 3. Costs are rounded upward from actual bid and construction cost.
 4. NIC – Not included in contract.

BEAUTIFICATION IMPROVEMENT OPINION OF COSTS AND CONSIDERATIONS FOR ROADWAYS

Collier Blvd. (C. R. 951), Part "A", ROADWAY #2
(Golden Gate Canal to Golden Gate Parkway, East)

Roadway length 1 mile, Medians green area length .9 mile, (4,750 l.f.)
Total median area - 71,250 s.f., Total right-of-way maintenance area - 157,000 s.f.

Typical landscape plan

Landscape & irrigation improvements complete.

Annual maintenance

Landscape & irrigation maintenance funded through Collier County roadway maintenance M.S.T.D. funds.

Roadway Improvements:

M.S.T.U. Cost

Pedestrian path, concrete	\$ <u>TBD</u>	(Facilities should be incorporated into roadway widening project)
Roadway decorative street lights	\$ <u>TBD</u>	(Street lighting should be incorporated into roadway widening projects)
Pedestrian decorative lights	\$ <u>TBD</u>	(Pedestrian fixtures should be incorporated into roadway widening project on median ends at pedestrian crossing locations)

- Note:**
1. Existing well and pump station will need to be abandoned upon roadway widening project.
 2. MSTU may be required to fund additional upgrade cost from County standard street light fixture to the decorative fixture.
 3. MSTU will need to provide funding for pedestrian decorative lighting, if installed.
 4. TBD – To be determined at time of design and installation.

BEAUTIFICATION IMPROVEMENT OPINION OF COSTS AND CONSIDERATIONS FOR ROADWAYS

**Collier Blvd. (C. R. 951), South, Part "B", ROADWAY #2
(Golden Gate Parkway, East to Green Blvd.)**

Roadway length 1 mile, Medians green area length .9 mile, (4,750 l.f.)

Total median area - 119,500 s.f., Planting area base on 95,000 s.f., Right-of-way maintenance area - 112,850 s.f.

Typical landscape plan

Landscape & irrigation improvements complete.

Annual maintenance

Landscape & irrigation maintenance funded through Collier County roadway maintenance M.S.T.D. funds.

Roadway Improvements:

M.S.T.U. Cost

Pedestrian path, <u>concrete</u>	\$ <u>TBD</u>	(Facilities should be incorporated into roadway widening project)
Roadway decorative <u>street</u> lights	\$ <u>TBD</u>	(Street lighting should be incorporated into roadway widening projects)
Pedestrian decorative lights	\$ <u>TBD</u>	(Pedestrian fixtures should be incorporated into roadway widening project on median ends at pedestrian crossing locations)

- Note:**
1. Existing well and pump station will need to be abandoned upon roadway widening project.
 2. MSTU may be required to fund additional upgrade cost from County standard street light fixture to the decorative.
 3. MSTU will need to provide funding for pedestrian decorative lighting, if installed.
 4. TBD – To be determined at time of design and installation.

BEAUTIFICATION IMPROVEMENT OPINION OF COSTS AND CONSIDERATIONS FOR ROADWAYS

GOLDEN GATE PARKWAY, East, ROADWAY #3

(Collier Blvd. (C.R. 951), West to Santa Barbara Blvd., North)

Roadway length 2.5 miles, Medians green area length 1.9 miles, (10,000 l.f.)

Total median area - 295,179 s.f., Total right-of-way maintenance area - 274,500 s.f.

Typical landscape plan

Landscape & irrigation improvements complete. (FY 88/89 THRU 94/95)

Annual maintenance

Landscape & irrigation maintenance funded through Collier County roadway maintenance M.S.T.D. funds.

Roadway Improvements:

M.S.T.U. Cost

Pedestrian path, concrete	\$ <u>TBD</u>	(Facilities should be incorporated into a roadway improvement project)
Roadway decorative street lights	\$ <u>TBD</u>	(Street lighting should be incorporated into roadway widening projects)
Pedestrian decorative lights	\$ <u>TBD</u>	(Pedestrian fixtures should be incorporated into median ends at pedestrian crossing locations)

- Note:**
1. Existing 8" well and 15 hp pump in medians #7 and #12 and 6" well and 10 hp pump in median #21.
 2. MSTU may be required to fund additional upgrade cost from County standard street light fixture to the decorative fixture.
 3. MSTU will need to provide funding for pedestrian decorative lighting, if installed.
(16 fixtures 2 at each intersections with Santa Barbara N., Coronado Pkwy., 47th St. SW, & 44th St. SW and 4 fixtures at 50th St. SW & Tropicana Blvd.)
 4. TBD - To be determined at time of design and installation.

BEAUTIFICATION IMPROVEMENT OPINION OF COSTS AND CONSIDERATIONS FOR ROADWAYS

GREEN BOULEVARD, ROADWAY #4

(Collier Blvd. (C. R. 951), South, Part "B" to Santa Barbara Blvd. North)

Roadway length 2 miles, Medians green area length .90 mile, (4,500 l.f.)

Total median area - 87,750 s.f., Total right-of-way maintenance area - 517,000 s.f.

This roadway corridor is currently an east-west urban collector highway between Collier Blvd. and Santa Barbara Blvd. North. County transportation planning has indicated the possibility of this roadway being developed into a minor arterial east-west highway. Based upon the current transportation planning trend landscape development of the roadway should occur with the future roadway widening or improvement project.

Landscape and irrigation development should be coordinated through the County's Roadway Beautification Master Plan program when the future roadway improvement project is planned.

Project Components

Estimated Costs

Design & Administration:	\$ <u>-0-</u>	(Landscape & Lighting Design)
Site Preparation and Soil Backfill:	\$ <u>-0-</u>	(Veg. removal, grading & topsoil)
Concrete Curbing:	\$ <u>-0-</u>	(FDOT Type "F" curb)
Decorative Paving:	\$ <u>-0-</u>	(Sand set Concrete pavers)
Pedestrian Facility	\$ <u>-0-</u>	(Per current County roadway policy)
Pedestrian Crosswalk Markings:	\$ <u>-0-</u>	(Surface applied reflective markings)
Pedestrian Accent Lighting:	\$ <u>TBD</u>	(Fixtures purchased directly)
Decorative Roadway Lighting:	\$ <u>TBD</u>	(Golden Gate Master Plan Fixtures)
Sleeving:	\$ <u>-0-</u>	(Irrigation & Electrical sleeves)
Landscape Plantings:	\$ <u>-0-</u>	
Irrigation System:	\$ <u>-0-</u>	(Well & pump station)
Sub-Total:	\$ <u>-0-</u>	
Contingency (10%):	\$ <u>-0-</u>	
Project Budget Total:	\$ <u>-0-</u>	

Annual Maintenance

Landscape & irrigation maintenance should be funded through Collier County roadway maintenance M.S.T.D. funds as an arterial roadway.

- Note:**
1. MSTU may be required to fund additional upgrade cost from County standard street light fixture to the decorative fixture.
 2. MSTU will need to provide funding for pedestrian decorative lighting, if installed.
 3. TBD - To be determined at time of design and installation.

BEAUTIFICATION IMPROVEMENT OPINION OF COSTS AND CONSIDERATIONS FOR ROADWAYS

HUNTER BOULEVARD, ROADWAY #5

(Coronado Parkway to Santa Barbara Blvd., North)

Roadway length .97 mile, Medians green area length .74 mile, (3,906 l.f.)

Total median area – 47,313 s.f., Total right-of-way maintenance area - 181,000 s.f.

<u>Project Components</u>	<u>FY 10/11 Hunter Blvd. & Coronado Pkwy. Combined Bid/Contract Pricing</u>
Design & Administration:	\$ 47,691.00 (Landscape, Engineering, Surveying & Lighting Design)
<u>Phase I Site Preparation & Curbing</u>	\$ 420,742.00
Site Preparation and Soil Backfill:	\$ <u>Included</u> (Veg. removal, grading & topsoil)
Concrete Curbing:	\$ <u>Included</u> (FDOT Type "F" curb)
Decorative Paving:	\$ <u>Included</u> (Sand set Concrete pavers)
Two Lane In-road Pedestrian Facility	\$ <u>Included</u> (Re-striping existing asphalt)
Pedestrian Crosswalk Markings:	\$ <u>Included</u> (Surface applied reflective markings)
Asphalt Paving:	\$ <u>Included</u> (Selective paving for drainage)
Pedestrian Accent Lighting:	\$ <u>NIC</u> (Fixtures purchased directly)
Decorative Roadway Lighting:	\$ <u>NIC</u> (Golden Gate Master Plan Fixtures)
Sleeving:	\$ <u>Included</u> (Irrigation & Electrical sleeves)
Landscape Plantings:	\$ 159,984.00
Irrigation System:	\$ <u>Included</u> (Well & pump station)
Sub-Total:	\$ 628,417.00
Contingency (10%):	\$ <u>-0-</u> (or as required by County Policy)
Project Budget Total:	\$ 628,417.00
 <u>Annual Maintenance</u>	 \$ 27,000.00 (Estimated)

- Note:**
1. The above costs are based upon actual FY 10 project construction bid and contract pricing.
 2. Maintenance costs derived from FY 09/10 County maintenance bid pricing.
 3. Costs are rounded upward from actual bid and construction cost.
 4. NIC – Not included in contract.

BEAUTIFICATION IMPROVEMENT OPINION OF COSTS AND CONSIDERATIONS FOR ROADWAYS

SANTA BARBARA BOULEVARD, NORTH, ROADWAY #6
(Golden Gate Canal to Green Blvd.)

Roadway length: Improved six lane section .81 miles, Unimproved four lane section 1.3 miles.
 Median green area lengths. Improved six lane section 4,270 ln. ft., Unimproved four lane section 6,764 ln. ft.
 Total median area – Improved six lane section 73,669 sq. ft., Unimproved four lane section (TBD s.f.)
 Total right-of-way maintenance area – (TBD s.f.)

This roadway corridor is a north-south urban arterial highway between Green Blvd. and the Golden Gate Canal. The landscaping for this roadway section is planned under the County’s Roadway Beautification Master Plan. Landscape, irrigation and lighting improvements should be coordinated through the County’s Roadway Beautification Master Plan program at the time the future roadway widening project are planned and constructed.

<u>Project Components</u>	<u>FY 10/11 Estimated Costs Based Upon Coronado Pkwy. & Hunter Blvd. Bid Pricing</u>		
	<u>Improved Six Lane</u>	<u>Unimproved Four Lane</u>	
Design & Administration:	\$ <u>19,500.00</u>	\$ <u>TBD</u>	(Landscape& Irrigation Design, Contract Administration)
Site Preparation and Soil Backfill:	\$ <u>10,000.00</u>	\$ <u>TBD</u>	(Veg. removal, grading & topsoil)
Concrete Curbing:	\$ <u>Existing</u>	\$ <u>TBD</u>	(FDOT Type “F” curb)
Decorative Paving:	\$ <u>Existing</u>	\$ <u>TBD</u>	(Sand set Concrete pavers)
Pedestrian Facility	\$ <u>Existing</u>	\$ <u>TBD</u>	(Per current County roadway policy)
Pedestrian Crosswalk Markings:	\$ <u>Existing</u>	\$ <u>TBD</u>	(Surface applied reflective markings)
Pedestrian Accent Lighting:	\$ <u>NIC</u>	\$ <u>-0-</u>	(Fixtures purchased directly)
Decorative Roadway Lighting:	\$ <u>NIC</u>	\$ <u>-0-</u>	(Golden Gate Master Plan Fixtures)
Sleeving:	\$ <u>Existing</u>	\$ <u>TBD</u>	(Irrigation & Electrical sleeves)
Landscape Plantings:	\$ <u>203,326.00</u>	\$ <u>TBD</u>	
Irrigation System:	\$ <u>Included</u>	\$ <u>TBD</u>	(Well & pump station)
Sub-Total:	\$ <u>-0-</u>	\$ <u>-0-</u>	
Contingency (10%):	\$ <u>-0-</u>	\$ <u>-0-</u>	(or as required by County policy)
Project Budget Total:	\$ <u>232,826.00</u>	\$ <u>TBD</u>	

Annual Maintenance

Landscape & irrigation maintenance should be funded through Collier County roadway maintenance M.S.T.D. funds as an arterial roadway.
 Improved Six Lane Section: \$ 38,000.00 (Estimated) Unimproved Four Lane Section: \$ TBD (Estimated)

Note: 1. TBD - To be determined at time of design and installation.

BEAUTIFICATION IMPROVEMENT OPINION OF COSTS AND CONSIDERATIONS FOR ROADWAYS

SUNSHINE BOULEVARD, ROADWAY #7

(Golden Gate Parkway, East to Green Blvd.)

Roadway length 1.1 miles, Medians green area length .78 mile, (4,118 l.f.)

Total median area - 69,135 s.f., Total right-of-way maintenance area - 205,000 s.f.

Typical landscape plan

Landscape & irrigation improvements complete. (FY 06/07 - \$662,000+/-)

Annual maintenance

Landscape, irrigation & lighting maintenance funded M.S.T.U. funds. (FY 10/11 - \$65,000)

Roadway Improvements:

M.S.T.U. Cost

Pedestrian path, concrete \$ -0- (Facilities should be incorporated into pathways improvement project)

Roadway decorative street lights \$ TBD (Street lighting should be incorporated using master plan decorative fixture)

- Note:**
1. MSTU may be required to fund additional upgrade cost from County standard street light fixture to the decorative fixture.
 2. TBD – To be determined at time of design and installation.

BEAUTIFICATION IMPROVEMENT OPINION OF COSTS AND CONSIDERATIONS FOR ROADWAYS

TROPICANA BOULEVARD, ROADWAY #8

(Golden Gate Parkway, East to 32nd Ave. SW)

Roadway length .80 mile, Medians green area length .70 mile, (3,696 l.f.)

Total median area - 55,059 s.f., Total right-of-way maintenance area - 152,000 s.f.

Roadway is striped as a two lane in-road bicycle/pedestrian facility

Typical landscape plan

Landscape & irrigation improvements complete. (FY 01/02 - \$416,000+/-)

Annual maintenance

Landscape, irrigation & lighting maintenance funded M.S.T.U. funds. (FY 10/11 - \$65,000)

Roadway Improvements:

M.S.T.U. Cost

Pedestrian path, concrete \$ -0- (Facilities should be incorporated into pathways improvement project)

Roadway decorative street lights \$ TBD (Street lighting should be incorporated using master plan decorative fixture)

- Note:**
1. MSTU may be required to fund additional upgrade cost from County standard street light fixture to the decorative fixture.
 2. TBD – To be determined at time of design and installation.

PHASING AND FUNDING SCHEDULES

Golden Gate Beautification Master Plan
Multiple Year Roadway Phasing & Funding Schedule

The phasing and funding schedule was developed to address the Collier County major arterial roadways within the M.S.T.U. first and then improve the collector roadways within the M.S.T.U. district. It is apparent these roadways are the Gateway roads within the Golden Gate Community, as well as for southeast Collier County and the Naples Urban Area. This phasing schedule is in keeping with the Master plan goals and original philosophy behind the original Golden Gate Parkway East roadway beautification project.

The phasing and funding schedule is based upon the concept to continue the public/private partnership with the Collier County Board of County Commissioners. This schedule is also consistent with the Collier County's past actions to assist in funding and implementation of roadway beautification within the Naples Urban Area on a case by case basis. The schedule is also consistent with the previously adopted "Collier County Streetscape Master Plan" and the recently adopted "Collier County Landscape Beautification Master Plan". The schedule is in keeping with the methodology, intent, and streetscape network of roadways to be improved by the above mentioned Master Plans. The methodology of the multiple year schedule is to assist the County in reducing the time frame and funding costs of the roadway landscape improvements by utilizing local residents tax dollars and volunteer services for administration of the projects. The multiple year schedule benefits the County and local residents by saving money and beautifying Collier County roadways.

The schedule costs do not take into account potential outside funding from grants or private local organizations.

M.S.T.U. Major Arterial Roadways

#1: Collier Blvd. (C.R. 951), Part "A"
(Golden Gate Canal to Golden Gate Parkway)

Implementation date: FY 1997-1998 **(Completed)**
Typical landscape plan: Type T5
Annual maintenance: Funded under the Collier County M.S.T.D.
Curbing: Existing curbed roadway.

#2: Collier Blvd. (C.R. 951), Part "B"
(Golden Gate Parkway to Green Blvd.)

Implementation date: FY 1998-1999 **(Completed)**
Typical landscape plan: Type T3 curbed
Annual maintenance: Funded under the Collier County M.S.T.D.

Curbing: Existing curbed roadway.

**#3: Santa Barbara Blvd., North
(Golden Gate Canal to end of Six Lane Improvements, Phase I)**

Implementation date: Undetermined, (To be implemented under Collier County Landscape Beautification Master Plan)

Typical landscape plan: Type T5

Annual maintenance: To be funded under the Collier County (M.S.T.D.) Transportation Services landscape maintenance budgets.

Curbing: Existing

**#4: Santa Barbara Blvd., North, Unimproved Four Lane Section
(End of Six Lane Improvements to Green Blvd., Phase II)**

Implementation date: Undetermined (To be Implemented under Collier County Landscape Beautification Master Plan)

Typical landscape plan: Type T5

Annual maintenance: To be funded under the Collier County (M.S.T.D.) Transportation Services landscape maintenance budgets.

Curbing: Installed with roadway improvement project.

M.S.T.U. Interior Collector Roadways

**#1: Tropicana Blvd.
(Golden Gate Canal to Golden Gate Parkway, East)**

Implementation date: FY 2002-2003 **(Completed)**

Typical landscape plan: Type T3 curbed

Lighting: Pedestrian accent fixture at median ends.

Annual maintenance: M.S.T.U. funded

Curbing: FDOT Type "D" medians only

Pathway Facilities: Existing Two Lane Road in-road pathway and 5' asphalt sidewalk along both rights-of-way.

**#2: Sunshine Blvd.
(Golden Gate Parkway to Green Canal)**

Implementation date: FY 2006-2007 **(Completed)**

Typical landscape plan: Type T3 curbed
Lighting: Pedestrian accent fixture at median ends.
Annual maintenance: M.S.T.U. funded
Curbing: FDOT Type "D" medians only
Pathway Facilities: Existing 5' asphalt & concrete sidewalk along both rights-of-way.

**#3: Hunter Blvd.
(Coronado Parkway to Santa Barbara Blvd. North)**

Implementation date: FY: 2010-2011 **(Under Construction)**
Typical landscape plan: Type T5
Annual maintenance: M.S.T.U. funded
Curbing: FDOT Type "F" medians only
Pathway Facilities: Existing Two Lane Road In-Road Pathway. Recommended 5' concrete sidewalk along south & west rights-of-way.

**#4: Coronado Parkway
(Golden Gate Parkway to Santa Barbara Blvd. North)**

Implementation date: FY: 2010-2011 **(Under Construction)**
Typical landscape plan: Type T5
Annual maintenance: M.S.T.U. funded
Curbing: FDOT Type "F" medians only
Pathway Facilities: Existing 5' asphalt & concrete sidewalks along both rights-of-way.

GOLDEN GATE M.S.T.U. COMMUNITY ROADWAYS BEAUTIFICATION MASTER PLAN

Table 10

MULTIPLE YEAR FUNDING AND PHASING SCHEDULE ESTIMATED PROJECTIONS (UNAUDITED)
 TAX REVENUES BASED UPON ONE HALF MILL RATE USING COUNTY F.Y. 11 TAXABLE PROPERTY VALUE @ 7/1/10 OF \$556,097,624.00

FISCAL YEAR	TAX REVENUE (+ 1% yearly) INCREASE	OPERATING COST (+ 3% yearly) INCREASE	COUNTY TRANSFERS (1 % yearly) INCREASE	STATE REQUIRED RESERVES (5% yearly) FIXED	RESERVES FOR CONTINGENCY (4% yearly)	IMPROVEMENT FUNDS YEARLY	IMPROVEMENT FUNDS TOTAL WITH CARRY FORWARD	PROPOSED PROJECT CONSTRUCTION COSTS (CORONADO PKWY. & HUNTER BLVD.)
FY 10	\$ 415,700	\$ 147,098	\$ 37,300	\$ 20,785	\$ 3,800	\$ -	\$ 1,350,279	
FY 11	\$ 278,049	\$ 151,511	\$ 37,673	\$ 13,902	\$ 12,000	\$ 74,962	\$ 369,376	\$ 1,055,865.00
FY 12	\$ 280,829	\$ 208,057	\$ 38,050	\$ 14,041	\$ 11,233	\$ 20,681	\$ 391,904	HUNTER/CORONADO MAINTENANCE ADDED
FY 13	\$ 283,638	\$ 214,298	\$ 38,430	\$ 14,182	\$ 11,346	\$ 16,727	\$ 410,591	
FY 14	\$ 286,474	\$ 220,727	\$ 38,815	\$ 14,324	\$ 11,459	\$ 12,608	\$ 425,252	
FY 15	\$ 289,339	\$ 227,349	\$ 39,203	\$ 14,467	\$ 11,574	\$ 8,320	\$ 435,698	
FY 16	\$ 292,232	\$ 234,170	\$ 39,595	\$ 14,612	\$ 11,689	\$ 3,856	\$ 441,733	
FY 17	\$ 295,154	\$ 241,195	\$ 39,991	\$ 14,758	\$ 11,806	\$ (789)	\$ 443,153	
FY 18	\$ 298,106	\$ 248,430	\$ 40,391	\$ 14,905	\$ 11,924	\$ (5,620)	\$ 439,749	
FY 19	\$ 301,087	\$ 255,883	\$ 40,794	\$ 15,054	\$ 12,043	\$ (10,645)	\$ 431,302	
FY 20	\$ 304,098	\$ 263,560	\$ 41,202	\$ 15,205	\$ 12,164	\$ (15,869)	\$ 417,589	
FY 21	\$ 307,139	\$ 271,467	\$ 41,614	\$ 15,357	\$ 12,286	\$ (21,299)	\$ 398,378	
FY 22	\$ 310,210	\$ 279,611	\$ 42,031	\$ 15,511	\$ 12,408	\$ (26,942)	\$ 373,428	
FY 23	\$ 313,312	\$ 287,999	\$ 42,451	\$ 15,666	\$ 12,532	\$ (32,803)	\$ 342,492	
FY 24	\$ 316,445	\$ 296,639	\$ 42,875	\$ 15,822	\$ 12,658	\$ (38,891)	\$ 305,314	
FY 25	\$ 319,610	\$ 305,538	\$ 43,304	\$ 15,980	\$ 12,784	\$ (45,213)	\$ 261,627	
FY 26	\$ 322,806	\$ 314,704	\$ 43,737	\$ 16,140	\$ 12,912	\$ (51,776)	\$ 211,160	
FY 27	\$ 326,034	\$ 324,145	\$ 44,175	\$ 16,302	\$ 13,041	\$ (58,588)	\$ 153,628	

NOTES: () - INDICATES NEGATIVE NUMBERS.

1. IMPROVEMENT FUNDS YEARLY INCLUDES RESERVES FOR CONTINGENCY.
2. DURING FY 17 IMPROVEMENT FUNDS YEARLY BECOMES A NEGATIVE NUMBER. TAX REVENUES EQUAL BUDGETED EXPENDITURES.
3. CORONADO PKWY. & HUNTER BLVD. PROJECT COSTS ARE BASED UPON FY 10/11 BID COSTS
4. ONE HALF A PERCENT INTEREST INCOME IS ADDED TO IMPROVEMENT FUNDS TOTAL PER YEAR.
5. OPERATING COSTS INCLUDES THE ANNUAL MSTU LANDSCAPE MAINTENANCE, PROFESSIONAL SERVICES, UTILITIES, INSURANCE & ADMIMISTRATIVE COSTS.

APPENDIX:

References

Americans with Disabilities Act of 1990

<http://www.ada.gov/>

Collier County

<http://www.colliergov.net/>

Land Development Code,

<http://www.municode.com/library/library.aspx>

Collier County Streetscape Master Plan

<http://www.colliergov.net/Index.aspx?page=29>

Construction in Public Rights-of-way Standards

<http://www.colliergov.net/Index.aspx?page=1415>

Golden Gate Area Master Plan

<http://www.colliergov.net/Index.aspx?page=257>

Neighborhood Traffic Management Program

Collier County Transportation Services

<http://www.colliergov.net/Index.aspx?page=570>

Collier County Development and Environmental Services

<http://www.colliergov.net/Index.aspx?page=103>

Comprehensive Planning Section

<http://www.colliergov.net/Index.aspx?page=96>

Transportation Engineering & Construction Management

<http://www.colliergov.net/Index.aspx?page=122>

Golden Gate Beautification Advisory Committee

<http://www.colliergov.net/Index.aspx?page=1454>

Metropolitan Planning Organization, M.P.O.

<http://www.colliercountympo.com/>

Pathways

<http://www.colliergov.net/Index.aspx?page=1476>

Florida Department of Agriculture and Consumer Services

<http://www.freshfromflorida.com/publications.html>

Division of Forestry

"Urban Trees for Florida" September 1980

Division of Plant Industry

"Grades and Standards for Nursery Plants", 3rd edition 1973

"Grades and Standards for Nursery Plants, Part II, Trees and Palms", 3rd edition, 1975

Florida Department of Transportation

F.D.O.T. Safety Home

<http://www.dot.state.fl.us/safety/>

Walking Safety Brochures

http://www.dot.state.fl.us/safety/ped_bike/brochures/ped_bike_brochures_walking.sht

F.D.O.T. Maps & Publications

"Roadway and Traffic Design Standards"

"Standard Specifications for Road and Bridge Construction"

<http://www.dot.state.fl.us/mapsandpublications/>

"Florida Highway Landscape Guide", April, 1995

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Florida Irrigation Society

<http://www.fisstate.org/>

"Standards and Specifications for Turf and Landscape Irrigation Systems", February, 1996

Florida Nurserymen and Growers Association, F.N.G.A.

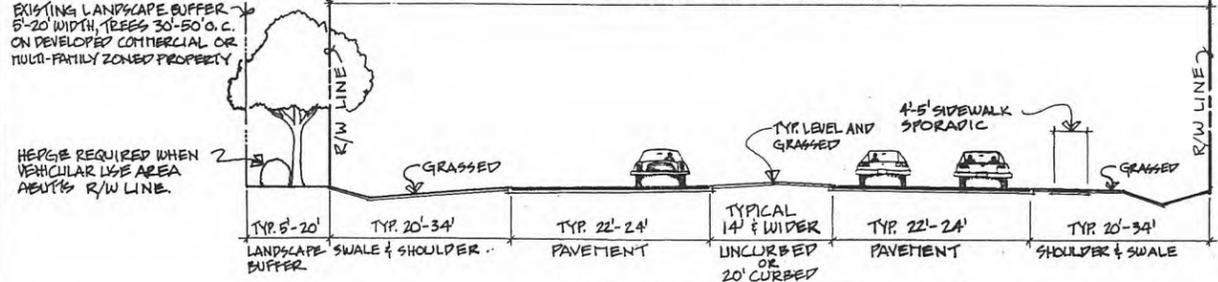
<http://www.fngla.org/certifications/FCHP>

"Florida Certified Horticulture Professional Manual"

South Florida Water Management District "WaterWise" Florida Landscapes

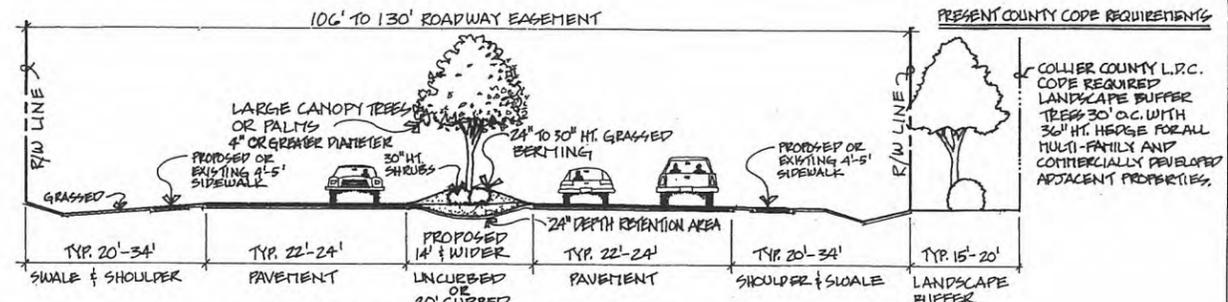
http://www.swfwmd.state.fl.us/publications/files/waterwise_landscapes.pdf

PAST COUNTY CODE REQUIREMENTS



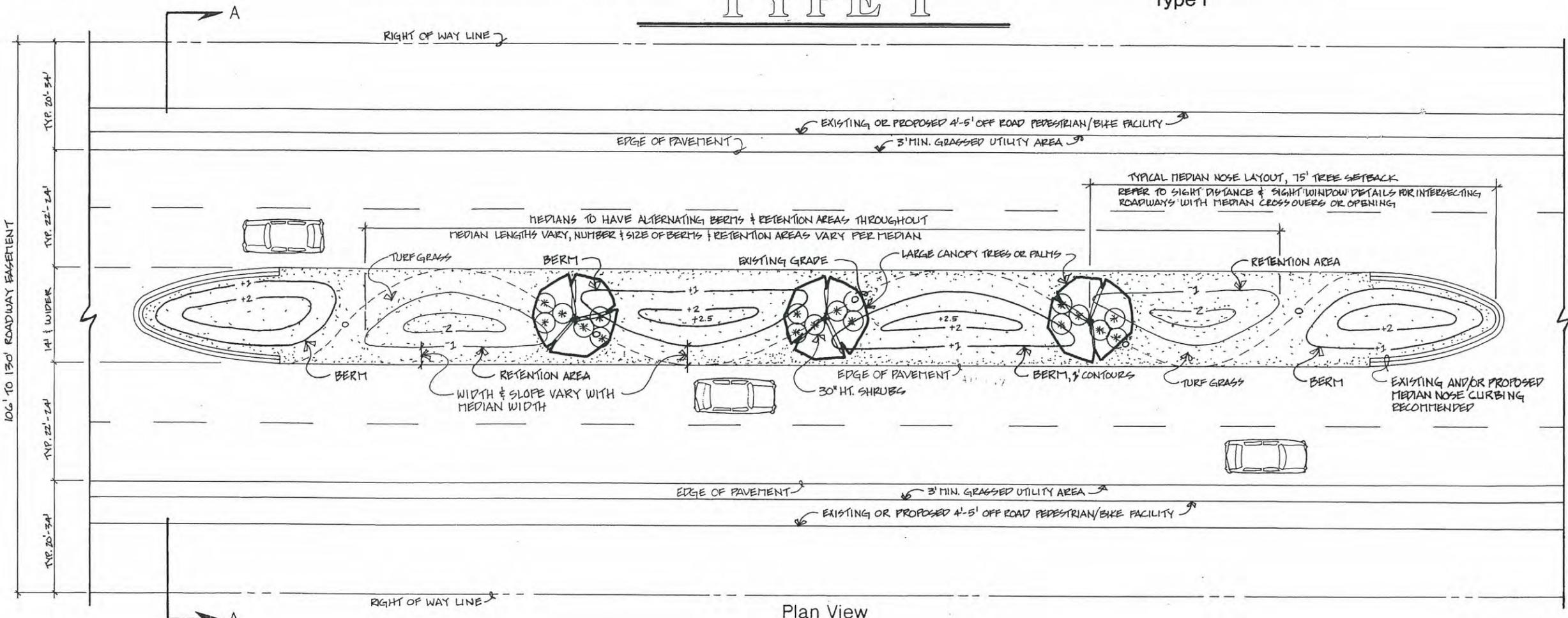
Typical Existing Section A-A

PRESENT COUNTY CODE REQUIREMENTS

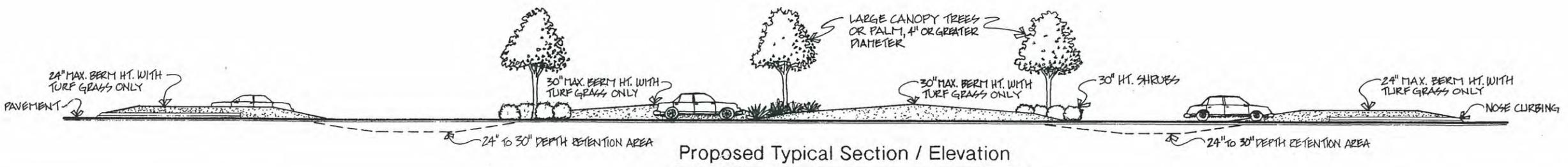


Proposed Typical Section A-A

TYPE I



Plan View
Type I



Proposed Typical Section / Elevation
Type I

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Golden Gate Beautification District
 Typical Roadway Landscape Plan
 (Uncurbed 14' or wider and curbed 20' or wider medians)

NOTES / REVISIONS:

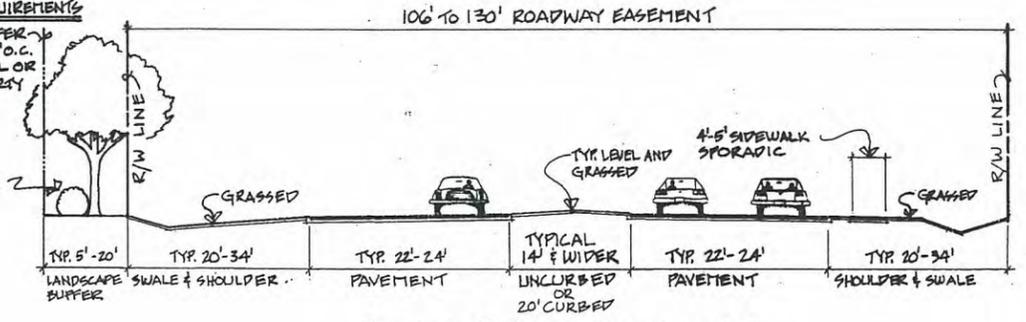
APPROVED BY: P.A.T.
 DRAWN BY: P.A.T.
 DATE: 12/10/98
 PROJECT NO. Florida
 SCALE: N.T.S.
 APPROVED BY: Collier County
 Michael A. McGee, F.L.S.
 Fla Reg No. LA 864
 Date:

McGee & Associates
 Landscape Architecture
 P. O. Box 8052
 Naples, Florida 34101
 (941) 263-0486
 LC 000098

PAST COUNTY CODE REQUIREMENTS

EXISTING LANDSCAPE BUFFER 5'-20' WIDTH, TREES 30'-60' O.C. ON DEVELOPED COMMERCIAL OR MULTI-FAMILY ZONED PROPERTY

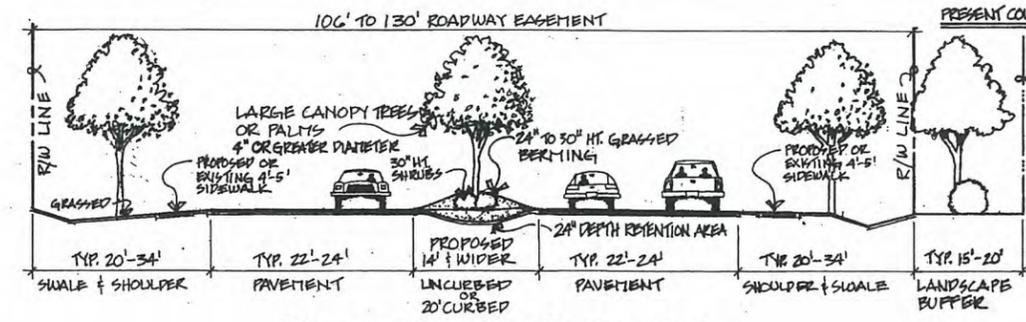
HEDGE REQUIRED WHEN VEHICULAR USE AREA ADJUTS R/W LINE



Typical Existing Section A-A

PRESENT COUNTY CODE REQUIREMENTS

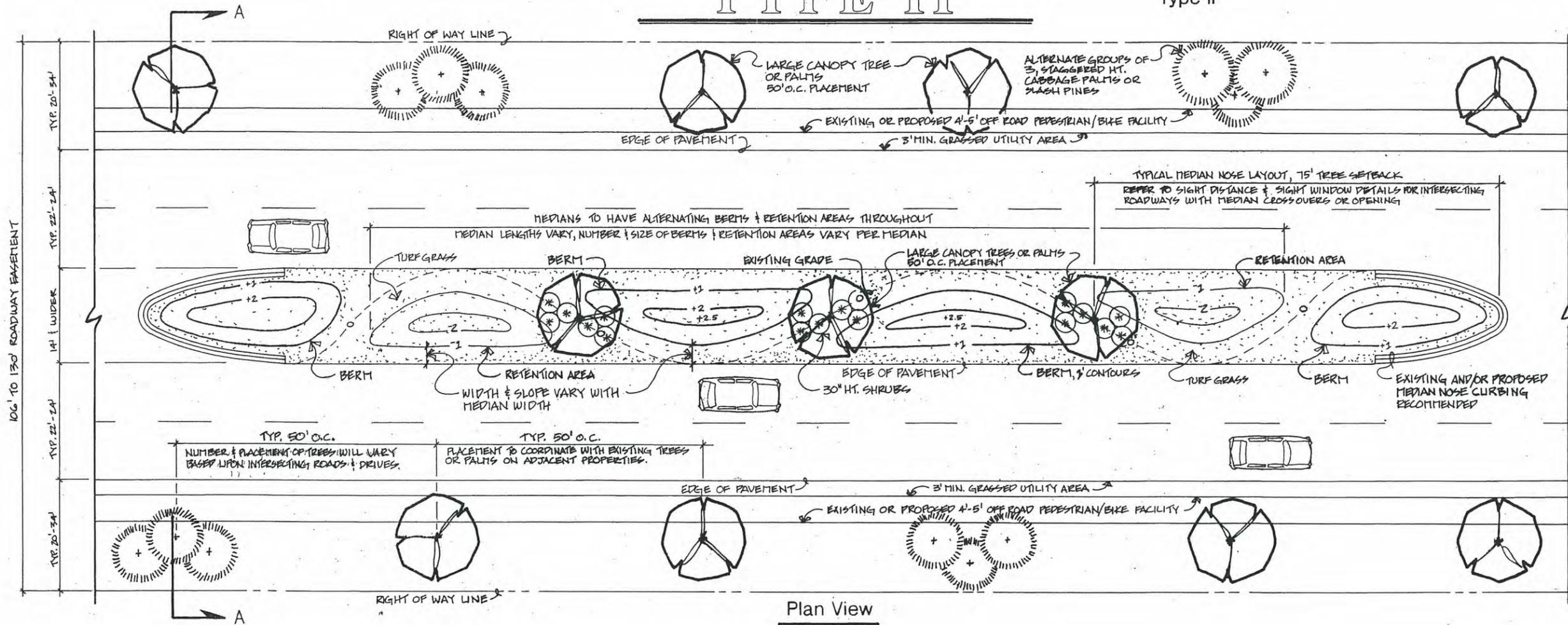
COLLIER COUNTY L.R.C. CODE REQUIRED LANDSCAPE BUFFER TREES 30' O.C. WITH 36" HT. HEDGE FOR ALL MULTI-FAMILY AND COMMERCIAL DEVELOPED ADJACENT PROPERTIES.



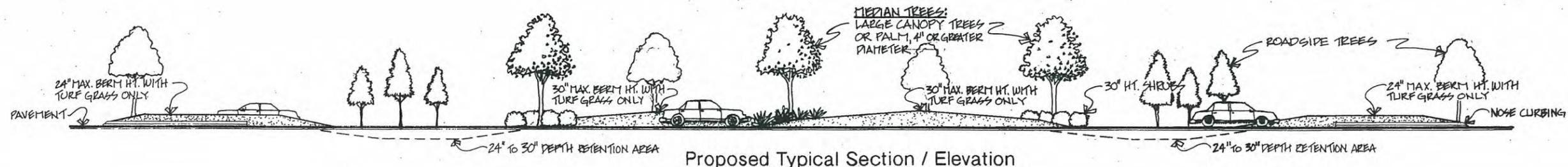
Proposed Typical Section A-A

TYPE II

Type II



Plan View
Type II



Proposed Typical Section / Elevation
Type II

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Golden Gate Beautification District
 Typical Roadway Landscape Plan
 (Uncurbed 14' or wider and curbed 20' or wider medians)

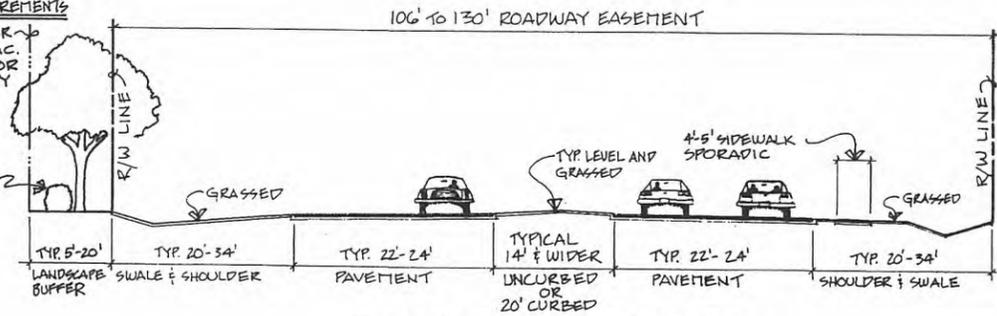
Project No. _____
 Date: 12/10/96
 Drawn by: F.L.A.T.
 Scale: N.T.S.
 Approved by: _____
 Michael A. McGee, F.L.A. 664
 P.L. Reg. No. _____
 Date: _____

McGee & Associates
 Landscape Architecture
 P. O. Box 8052
 Naples, Florida 34101
 (941) 263-0486

PAST COUNTY CODE REQUIREMENTS

EXISTING LANDSCAPE BUFFER 5'-20' WIDTH, TREES 30'-50' O.C. ON DEVELOPED COMMERCIAL OR MULTI-FAMILY ZONED PROPERTY

EDGE REQUIRED WHEN VEHICULAR USE AREA ADJUTTS R/W LINE

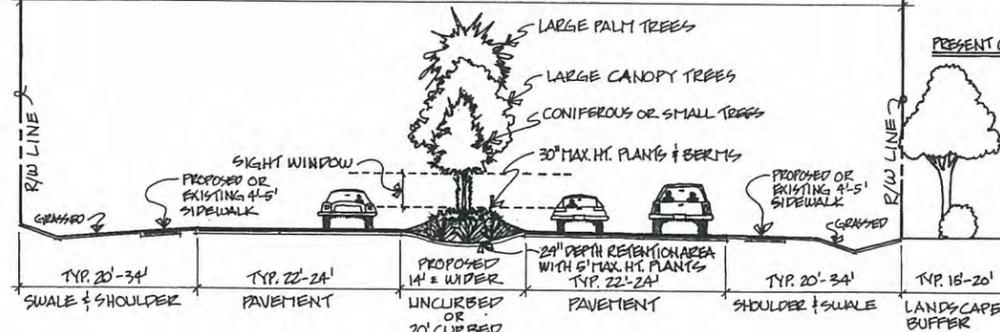


Typical Existing Section A-A

106' TO 130' ROADWAY EASEMENT

PRESENT COUNTY CODE REQUIREMENTS

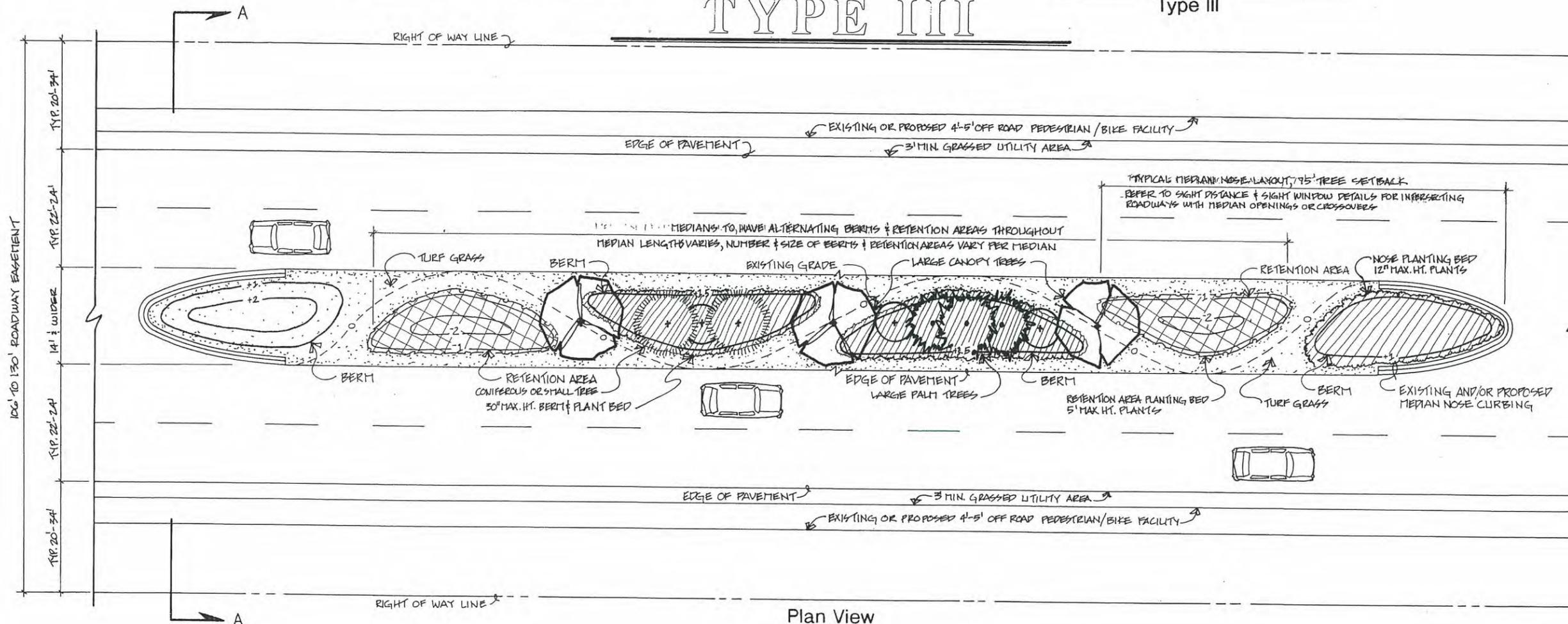
COLLIER COUNTY L.R.C. CODE REQUIRED LANDSCAPE BUFFER TREES 30' O.C. WITH 36" HT. HEDGES FOR ALL MULTI-FAMILY AND COMMERCIAL DEVELOPED ADJACENT PROPERTIES.



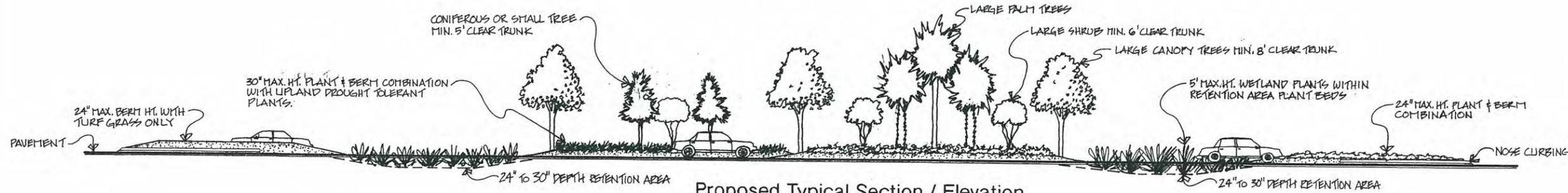
Proposed Typical Section A-A

TYPE III

Type III



Plan View
Type III



Proposed Typical Section / Elevation
Type III

NOTES / REVISIONS:

Michael A. McGee, P.L.A.
L.A. 864
Date:

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Naples, Florida 34101
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LC 000098

Golden Gate Beautification District
Typical Roadway Landscape Plan
(Uncurbed 14' or wider and curbed 20' or wider medians)
Collier County
N.T.S.
DRAWN BY: P.A.T.
DATE: 12/10/96
PROJECT NO. Florida

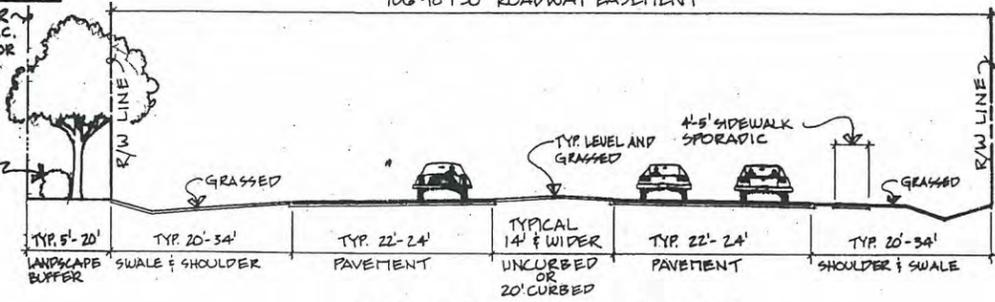
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PAST COUNTY CODE REQUIREMENTS

EXISTING LANDSCAPE BUFFER 3'-20' WIDTH, TREES 30'-50' O.C. ON DEVELOPED COMMERCIAL OR MULTI-FAMILY ZONED PROPERTY

HEDGE REQUIRED WHEN VEHICULAR USE AREA ADJACENT R/W LINE

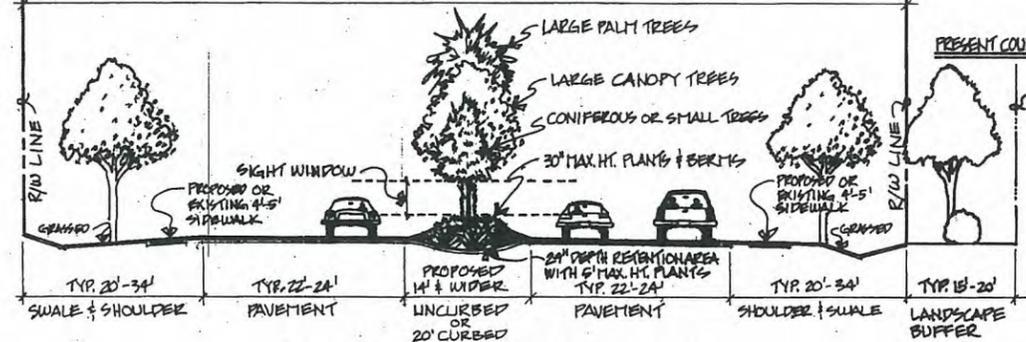


Typical Existing Section A-A

106' TO 130' ROADWAY EASEMENT

PRESENT COUNTY CODE REQUIREMENTS

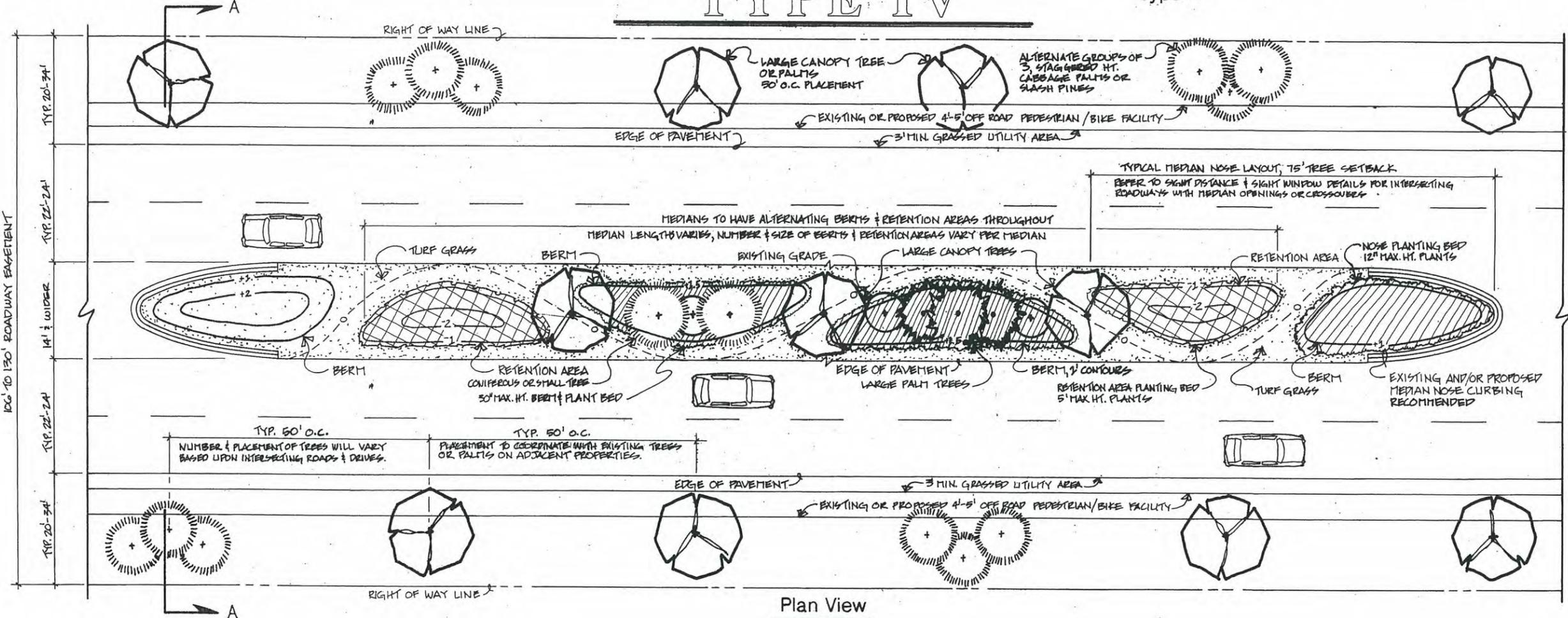
COLLIER COUNTY L.P.C. CODE REQUIRED LANDSCAPE BUFFER TREES 30' O.C. WITH 36" HT. HEDGE FOR ALL MULTI-FAMILY AND COMMERCIAL DEVELOPMENT ADJACENT PROPERTIES.



Proposed Typical Section A-A

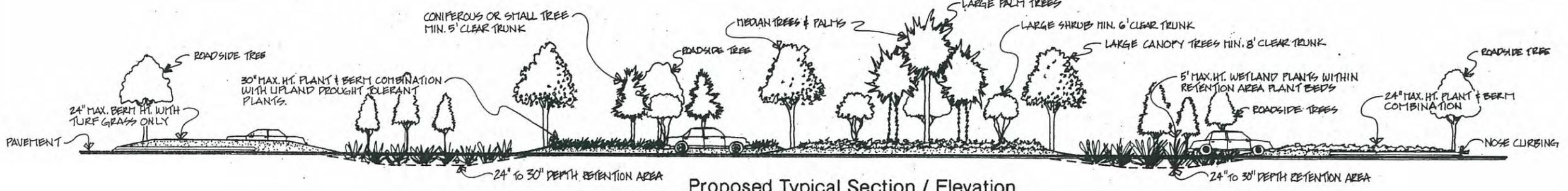
TYPE IV

Type IV



Plan View

Type IV



Proposed Typical Section / Elevation

Type IV

T 4

Golden Gate Beautification District
 Typical Roadway Landscape Plan
 (Uncurbed 14' or wider and curbed 20' or wider medians)

PROJECT NO. _____
 DATE: 12/10/96
 DRAWN BY: P.A.T.
 APPROVED BY: _____
 SCALE: N.T.S.
 NOTES / REVISIONS:

Michael A. McGee, r.l.s.
 Fla. Reg. No. L.A. 864
 Date: _____

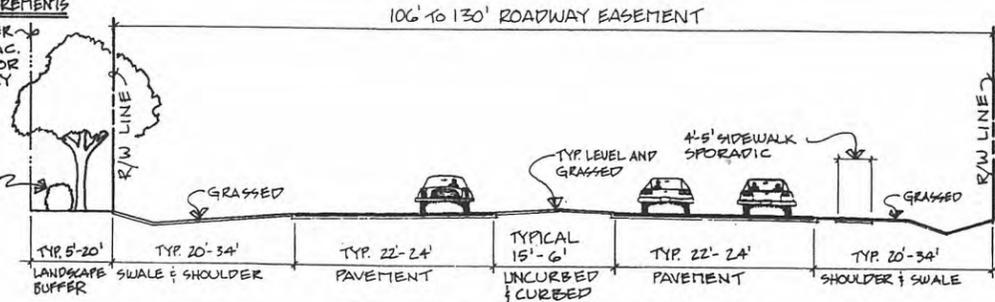
McGee & Associates
 Landscape Architecture
 P. O. Box 8052
 Naples, Florida 34101
 (941) 263-0486
 L.C. 000088

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PAST COUNTY CODE REQUIREMENTS

EXISTING LANDSCAPE BUFFER 5'-20' WIDTH, TREES 50'-50' O.C. ON DEVELOPED COMMERCIAL OR MULTI-FAMILY ZONED PROPERTY

HEDGE REQUIRED WHEN VEHICULAR USE AREA ADJUTS R/W LINE

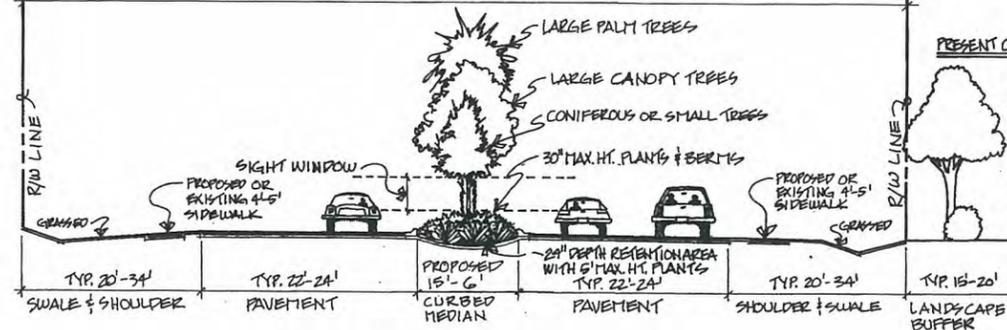


Typical Existing Section A-A

106' TO 130' ROADWAY EASEMENT

PRESENT COUNTY CODE REQUIREMENTS

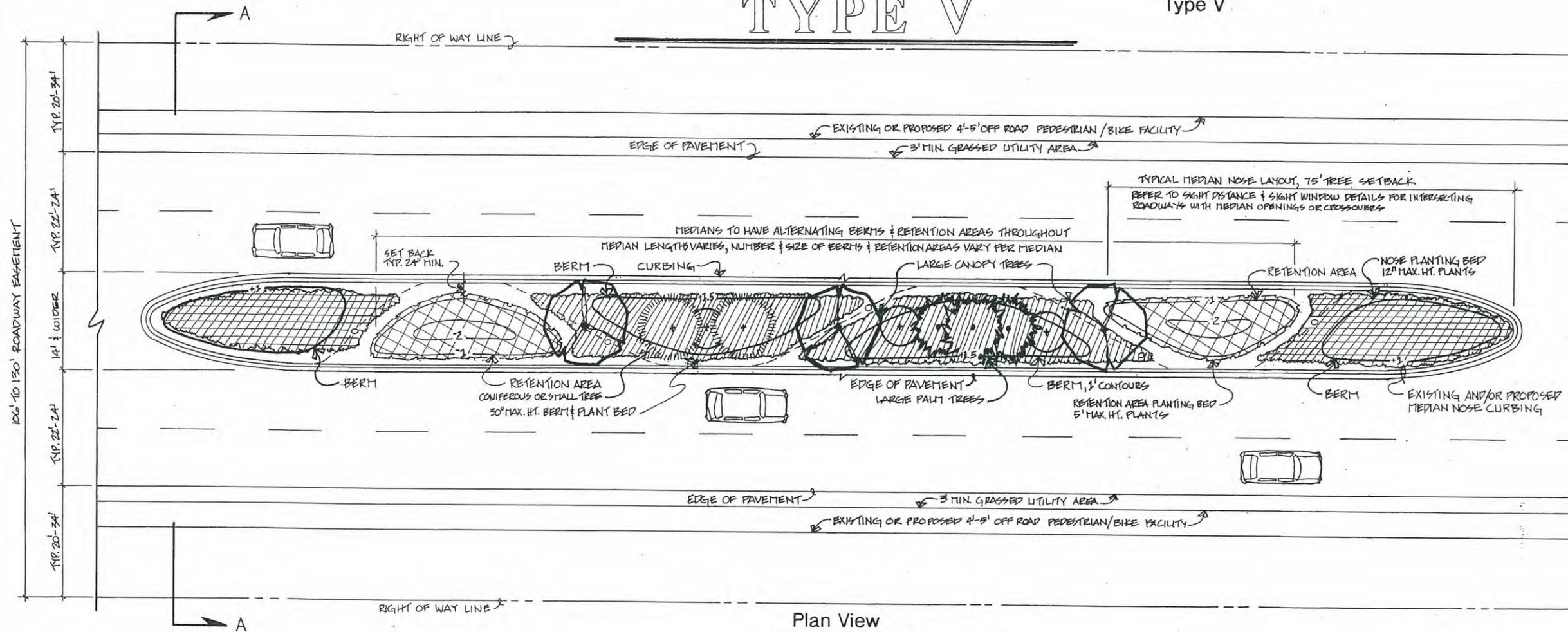
COLLIER COUNTY L.P.C. CODE REQUIRED LANDSCAPE BUFFER TREES 30' O.C. WITH 36" HT. HEDGE FOR ALL MULTI-FAMILY AND COMMERCIAL DEVELOPED ADJACENT PROPERTIES.



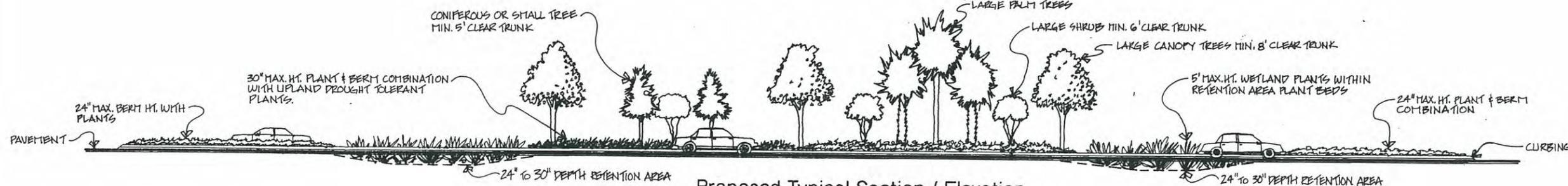
Proposed Typical Section A-A

TYPE V

Type V



Plan View
Type V



Proposed Typical Section / Elevation
Type V

Golden Gate Beautification District
Typical Roadway Landscape Plan
(Curbed medians 15' to 8')

NOTES / REVISIONS:

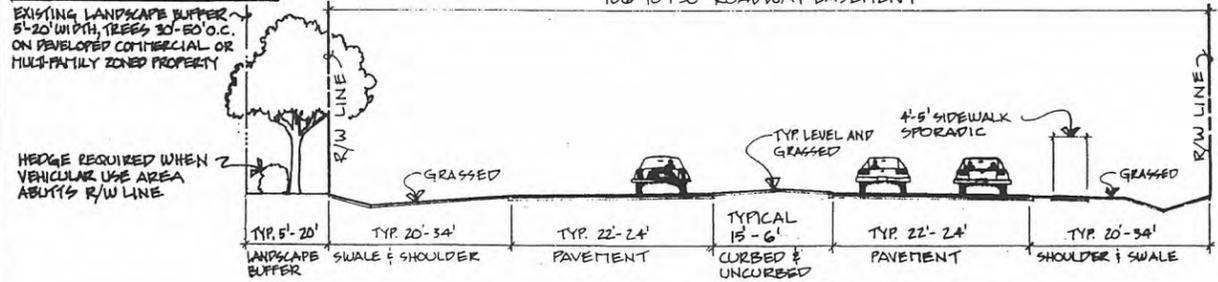
APPROVED BY: [Signature]
DATE: 12/10/96
SCALE: N.T.S.
DRAWN BY: P.A.T.
PROJECT NO. Florida

Michael A. McGee, P.L.C.
P.L. Reg. No. LA 864
Date:

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(941) 263-0486
LC 000098

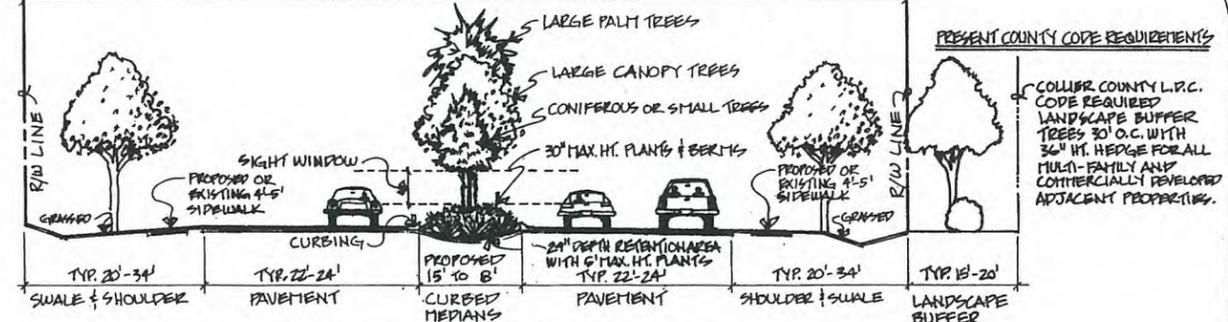
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PAST COUNTY CODE REQUIREMENTS



Typical Existing Section A-A

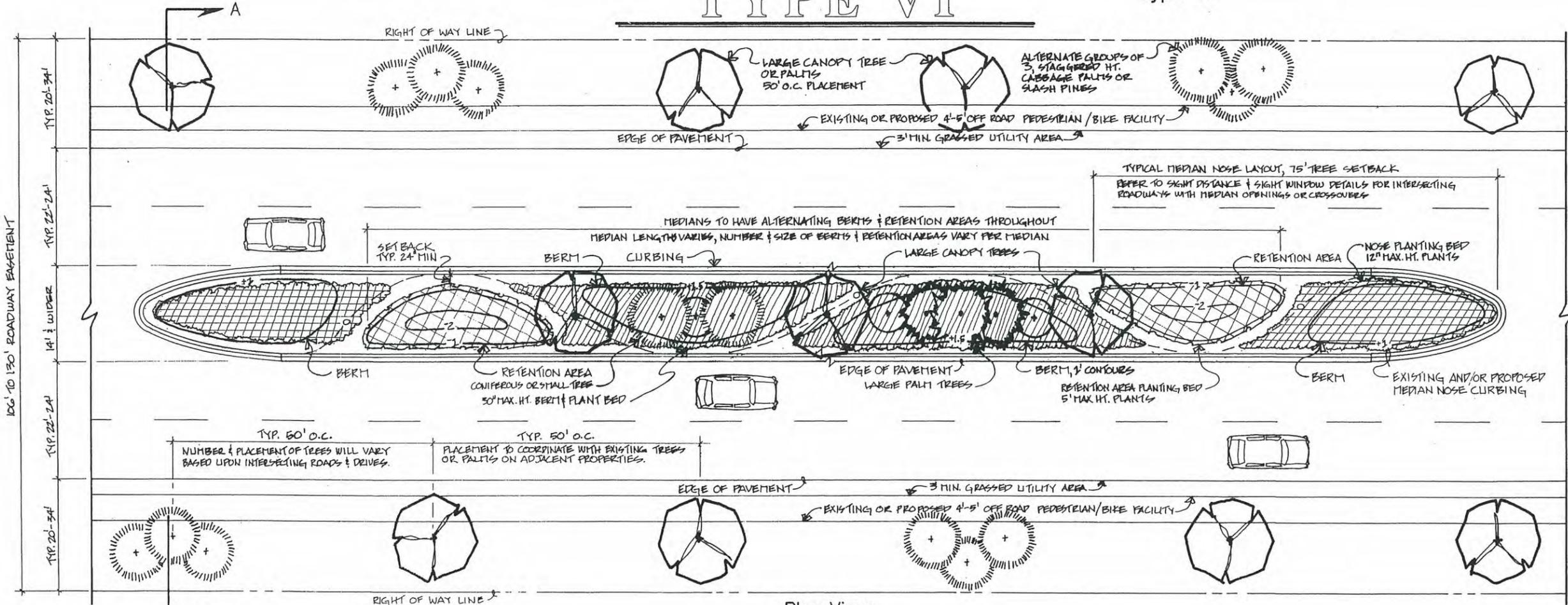
100' TO 130' ROADWAY EASEMENT



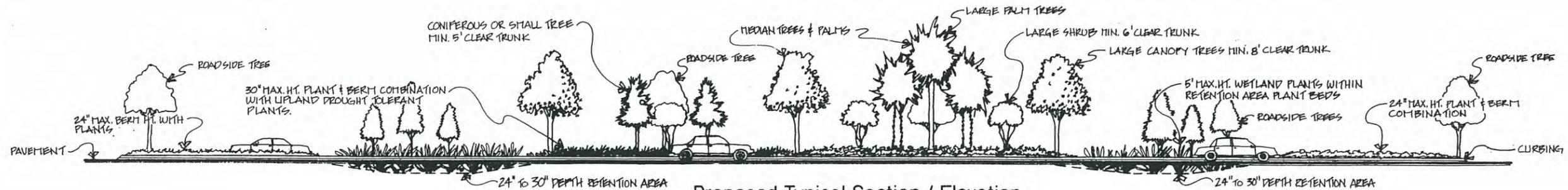
Proposed Typical Section A-A

TYPE VI

Type VI



Plan View
Type VI



Proposed Typical Section / Elevation
Type VI

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 P. O. Box 8052
 Naples, Florida 34101
 (941) 263-0486
 LC 0000298

NOTES / REVISIONS:

MICHAEL A. MCGEE, P.L.C.
 Fla. Reg. No. LA 564
 Date: _____

Golden Gate Beautification District
 Typical Roadway Landscape Plan
 (Curbed medians 15' to 6')

SCALE: N.T.S.
 DRAWN BY: P.A.F.
 DATE: 12/10/96
 PROJECT NO.: Florida

T 6